

Tom Davies Square
200 Brady Street
Sudbury, Ontario P3A 5P3

September 3, 2025

PUBLIC HEARINGS

PL-MV-2025-00107

**BRIAN MACLEAN
KATHLEEN STOKES**

Ward: 10

PIN(s) 735940052, Parcel 8570 SEC SES, Lot 28, Plan M-143, Part Lot 5, Concession 1, Township of McKim, 200 Maki Avenue, Sudbury, [By-law 2010-100Z, R1-5]

For relief from Part 4, Section 4.2, subsections 4.2.10.3 (e) and (i) and Part 5, Section 5.5, Table 5.5, of By-law 2010-100Z, being the Zoning By-law for the City of Greater Sudbury, as amended, to permit the conversion of the existing detached garage to an additional dwelling unit providing, firstly, the location of the additional dwelling unit within the front yard, where an additional dwelling unit located in a building accessory to a main building is only permitted within the rear and/or interior side yards, and secondly, no parking for the additional dwelling unit or existing single detached dwelling, where an additional dwelling unit located in a building accessory to a main building shall provide one additional parking space per additional dwelling unit that what would have been required on the property and where 1 parking space is required per unit.

PL-MV-2025-00110 CATHERINE DUPUIS

Ward: 1

PIN(s) 735860052, Part Lot 22, Plan 29-SB as in S70552, Part Lot 7, Concession 3, Township of McKim, 579 Elm Street, Sudbury, [By-law 2010-100Z, R2-2]

For relief from Part 4, Section 4.2, Table 4.1, Section 4.15, subsections 4.15.1 (e) and 4.15.2, Section 4.25, subsection 4.25.1, Part 5, Section 5.4, subsection 5.4.2 (d), Section 5.5, Table 5.5 and Part 6, Section 6.3, Table 6.3 of By-law 2010-100Z, being the Zoning By-law for the City of Greater Sudbury, as amended, to facilitate the construction of a duplex dwelling on existing footings with an attached uncovered deck providing, firstly, an uncovered deck 2.9m in height to encroach into the rear yard providing a 3.0m setback from the rear lot line and to encroach into the interior side yard providing a 0.9m setback from the interior side lot line, where uncovered decks greater than 1.2m in height may encroach 3.6 m into the required rear yard but not closer than 3.0 m to the rear lot line and 1.2 m into the required interior yard but no closer than 1.2 m to the interior side lot line, secondly, no landscaped area adjacent to Elm Street, where a 3.0m-wide landscaped area adjacent to the full length of a lot line shall be required abutting all public roads having a width greater than 10.0m in all Zones, thirdly, no landscaped open space in the required front yard, where a minimum of 50% of all required front yards shall be maintained as landscaped open space, fourthly, the required parking area in the required front yard to be a maximum width of 11.938m (100% of the width) of the minimum lot frontage, where outdoor parking areas are permitted in the required front yard to a maximum of 50% of the width of the minimum lot frontage, or the maximum driveway width established by this by-law, whichever is lesser, fifthly, 1 parking space, where 2 parking spaces are required, sixthly, a minimum lot area of 100.0sq. m per unit, where 230.0sq. m per unit is required, seventhly, a minimum lot frontage of 11.9m, where 15.0m is required, eighthly, a minimum front yard setback of 3.0, where 15.0m is required, ninthly, a minimum rear yard setback of 6.4, where 7.5m is required, tenthly, a minimum west interior side yard setback of 0.9m, where 1.8m is required, and eleventhly, a minimum east interior side yard setback of 1.2m, where 1.8m is required.

**PL-MV-2025-00112 NICOLE ROBICHAUD
BERNARD ROBICHAUD**

Ward: 6

PIN(s) 735041487, Parcel 37694 SEC SES SRO, Part Lot 284, Plan M-641, Part 3, Plan 53R-6936, Part Lot 5, Concession 2, Township of Hanmer, 1076 Pine Court, Hanmer, [By-law 2010-100Z, R1-5]

For relief from Part 4, Section 4.2, Table 4.1, Section 4.15, subsections 4.15.1 (e) and 4.15.2 and Part 6, Section 6.3, Table 6.2 of By-law 2010-100Z, being the Zoning By-law for the City of Greater Sudbury, as amended, to facilitate the construction of detached garage providing, firstly, a front yard setback of 2.43m with eaves encroaching 0.51m into the proposed 2.43m front yard, where a 6.0m front yard setback is required and where eaves may encroach 1.2m into the required front yard but not closer than 0.6m to the lot line, secondly, a 2.4m wide landscaped open space adjacent to the full length of the lot line abutting Pine Court, where a 3.0m wide landscaped area adjacent to the full length of the lot line shall be required abutting all public roads having a width greater than 10.0m, and thirdly, a minimum of 36% landscaped open space, where a minimum of 50% of all required front yards shall be maintained as landscaped open space in low density residential one zones.

PREVIOUSLY SUBJECT TO MINOR VARIANCE APPLICATIONS
A0223/1976 (NOV 15/76) AND A0092/2009 (AUG 10/09) AND CONSENT
APPLICATION B0751/1976 (NOV 15/76)

PL-MV-2025-00114 **CHRIS MORGAN**
NICOLE MORGAN

Ward: 3

PIN(s) 733520720, Parcel 25660 SEC SWS SRO, Lot 17, Plan M-1041, Part Lot 2, Concession 4, Township of Dowling, 60 Marcel Street, Dowling, [By-law 2010-100Z, R1-5]

For relief from Part 4, Section 4.2, subsections 4.2.3, and 4.2.10.3 (e) of By-law 2010-100Z, being the Zoning By-law for the City of Greater Sudbury, as amended, to facilitate the construction of a detached accessory building containing an additional dwelling unit providing, firstly, an accessory lot coverage of 18%, where the total lot coverage of all accessory buildings and structures on a residential lot shall not exceed 10%, and secondly, the location of the detached accessory building within the exterior yard, where an additional dwelling unit located in a building accessory to a main building is only permitted within the rear and/or interior side yards.

PL-MV-2025-00118 **JOSEE ROBERT**
JAMES ROBERT

Ward: 4

PIN(s) 733471440, 733471438, SRO, Part Lot 10, Concession 3, as in LT5771 and LT19549, except LT17713, LT60738, LT125350, LT128292, LT146628, Parts 1-3, Plan 53R-5475, Parts 1-4, Plan 53R-8389, Parts 1-7, Plan 53R-18049, Township of Rayside, 4176 Municipal Road 35, Chelmsford, [By-law 2010-100Z, RU]

For relief from Part 4, Section 4.2, subsection 4.2.4 b) of By-law 2010-100Z, being the Zoning By-law for the City of Greater Sudbury, as amended, to facilitate the construction of a detached garage providing a maximum height of 8.0m, where the height of any building or structure accessory to a residential dwelling shall be 6.5 metres.

PREVIOUSLY SUBJECT OF CONSENT APPLICATIONS B66/05 (JULY 27/05), B149/94 (JUNE 20/94), B627/77 TO B629/77 (OCTOBER 31/77) AND B96/73 & B97/73 (JULY 3/74)

This application was deferred from the meeting of June 25, 2025 in order to afford the owner the opportunity to address those comments received from agencies and departments.

**PL-MV-2025-00072 SAHIL BHARDWAJ
NEELIMA BHARDWAJ**

Ward: 11

PIN(s) 735780119, Parcel 5878 SEC SES, Part Lot 12, Concession 3, Parts 11-13, Plan 53R-13566, together with easement over Parts 2, 3, 5, 6, 8 & 10, Plan 53R-13566, Township of Neelon, 1827 Torbay Road, Sudbury, [By-law 2010-100Z, R1-5]

REVISED

For relief from Part 4, Section 4.41, subsections 4.41.2 and 4.41.4 of By-law 2010-100Z, being the Zoning By-law for the City of Greater Sudbury, as amended, to facilitate the construction of an addition on the existing single detached dwelling and to permit a shed providing high water mark setbacks of 17.9m for the addition and 13.7m for Garden Shed A as identified on the Site Plan, where accessory buildings or structures shall be no closer than 30.0m to the high water mark and where the only permitted structures within 20.0m of the high water mark are the permitted accessory structures set out in 4.41.2 of the Zoning By-law, boat launches, marine railways, waterlines and heat pump loops.

PREVIOUSLY SUBJECT TO CONSENT APPLICATIONS B0490/1990 (DEC 3/90) AND B0242/1991 (JUL 15/91)

A reminder... the next scheduled meeting is Wednesday, September 17, 2025.



Box 5000, Station A
200 Brady Street
Sudbury, Ontario P3A 5P3
(705) 671-2489 ext 4376 or 4346
(705) 673-2200 FAX

Record #: PL-MV-2025-00107

APPLICATION SUMMARY

File Date: 07/18/2025

Application Type: Minor Variance

Address(es): 200 Maki Avenue, 200 Maki Avenue, Sudbury P3E 2P2

Applicant(s): NORTH SOUTH STUDIO

Owner(s): BRIAN MACLEAN AND KATHLEEN STOKES

PLANNING APPLICATION

Are there multiple properties associated with the application?

No

Please describe the additional properties associated with this application

What is the date the current Owner(s) acquired the property?

+10 years

Are you the registered owner or an authorized agent?

Authorized Agent

What is the number of dwelling units on the property?

1

What is the number of proposed new dwelling units on the property?

1

What is the number of proposed new buildings/structures on the property?

0

What is the number of existing buildings/structures on the property?

2

If this application is approved, would any existing dwelling units be legalized?

No

How many dwelling units will be legalized?

Is this property located within an area subject to the Greater Sudbury Source Protection Plan?

No

Provide details on how the property is designated in the Source Protection Plan

Current Official Plan designation

Living Area I

Current Official Plan designation (additional)

Living Area I

Current Zoning By-law designation

R1-5

Provide a detailed description of what is being proposed

To convert a detached accessory garage to a accessory dwelling

Provide a detailed reason why the proposal cannot comply with the Zoning By-law

To permit no parking spaces where 2 spaces are required by by-law 2010-100z.

The proposed accessory dwelling unit cannot fully comply with the provisions of Zoning By-law 2010-100Z, as it is intended to be located within an existing legal non-conforming accessory structure on a heavily sloped waterfront property that is already fully developed. Given the topography of the site, the established landscaping, and the presence of mature trees, demolition and reconstruction would not be practical or desirable from an environmental or planning perspective.

With respect to parking, the property does not currently provide legal on-site parking due to the steep slope and the established character of this older section of Maki Avenue. This condition is consistent with many neighbouring properties along the street. As is common in this area, parking is accommodated off-site within the municipal road allowance between the front property boundary and the edge of the travelled roadway. At this location, sufficient width exists to accommodate and maintain the two required parking spaces in accordance with the intent of the Zoning By-law.

Since the development proposes no new construction and represents strictly a change of use within an existing building, there will be no adverse impacts to the character of the property, the surrounding neighbourhood, or the waterfront setting. The proposal maintains the general intent and purpose of both Zoning By-law 2010-100Z and the City's Official Plan by supporting compatible infill housing opportunities while respecting existing neighbourhood form and character.

Furthermore, the development will contribute to the supply of affordable housing, a priority strongly supported by the City of Greater Sudbury and the Province of Ontario. The Provincial Policy Statement (2020) and the Growth Plan for Northern Ontario both emphasize the importance of increasing housing supply through intensification and infill opportunities. This proposal directly aligns with those policies by creating a new, self-contained dwelling unit without requiring additional land consumption, servicing expansion, or significant site alteration.

Accordingly, the proposed change of use is consistent with the Provincial Policy Statement, conforms to the City of Greater Sudbury Official Plan, and meets the general intent of Zoning By-law 2010-100Z. It represents good planning by balancing the protection of existing neighbourhood character and environmental features with the provincial and municipal mandate to provide new affordable housing options in appropriate infill locations.

Is there an eave encroachment?

No

Size of eaves

Lot Frontage of the property

15.25

Lot Depth of the property

38.55

Lot Area of the property

628.7

Total width of the public road giving access to the property

20

List all buildings and structures on the property and their respective date of construction

Main dwelling - Construction unknown

Detached garage - construction unknown

Existing use(s) of the subject property and length of time it/they have continued, ex. Residential, Commercial, Institutional, Park, etc.

Residential

Is the use remaining the same? If no, please provide the proposed new use

Yes

Existing uses of neighbouring properties

Residential

Has the property ever been subject of a previous application for minor variance/permission?

No

Do you require zoning relief from Section 4.41 (Waterbodies – Water Frontage, Setbacks and Buffers)?

No

Have you consulted with the Strategic and Environmental Planning department regarding this relief?

Have you consulted with Conservation Sudbury regarding this relief?

WATER SUPPLY AND SEWAGE DISPOSAL

- Municipally owned and operated piped water system
- Municipally owned and operated sanitary sewage system
- Lake
- Pit Privy
- Individual Well
- Communal Well
- Individual Septic System
- Communal Septic System
- Other
- Explain Other

PROPERTY ACCESS

- Provincial highway
- Right-of-way
- Municipal road that is maintained seasonally
- Municipal road that is maintained year-round
- Water

Indicate the parking and docking facilities to be used if via water

Estimate the distance of these facilities from the retained land and nearest public road by water

CONCURRENT APPLICATIONS

Is the property the subject of a current application for Consent?
No

Indicate the application number(s) and status of the application(s)

Is the property the subject of a current application for a Plan of Subdivision?
No

Indicate application number(s) and application status

PROPOSED BUILDING/STRUCTURE

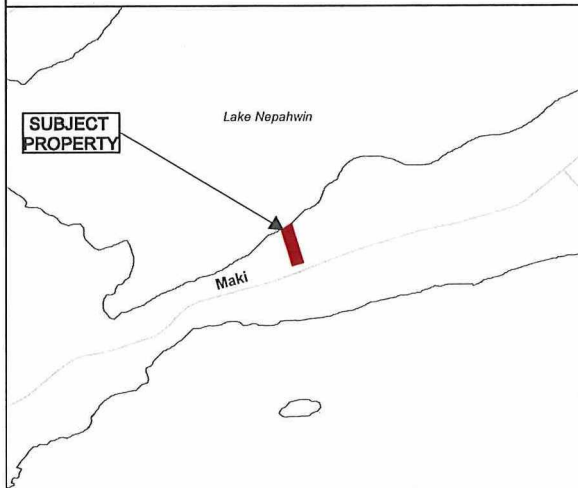
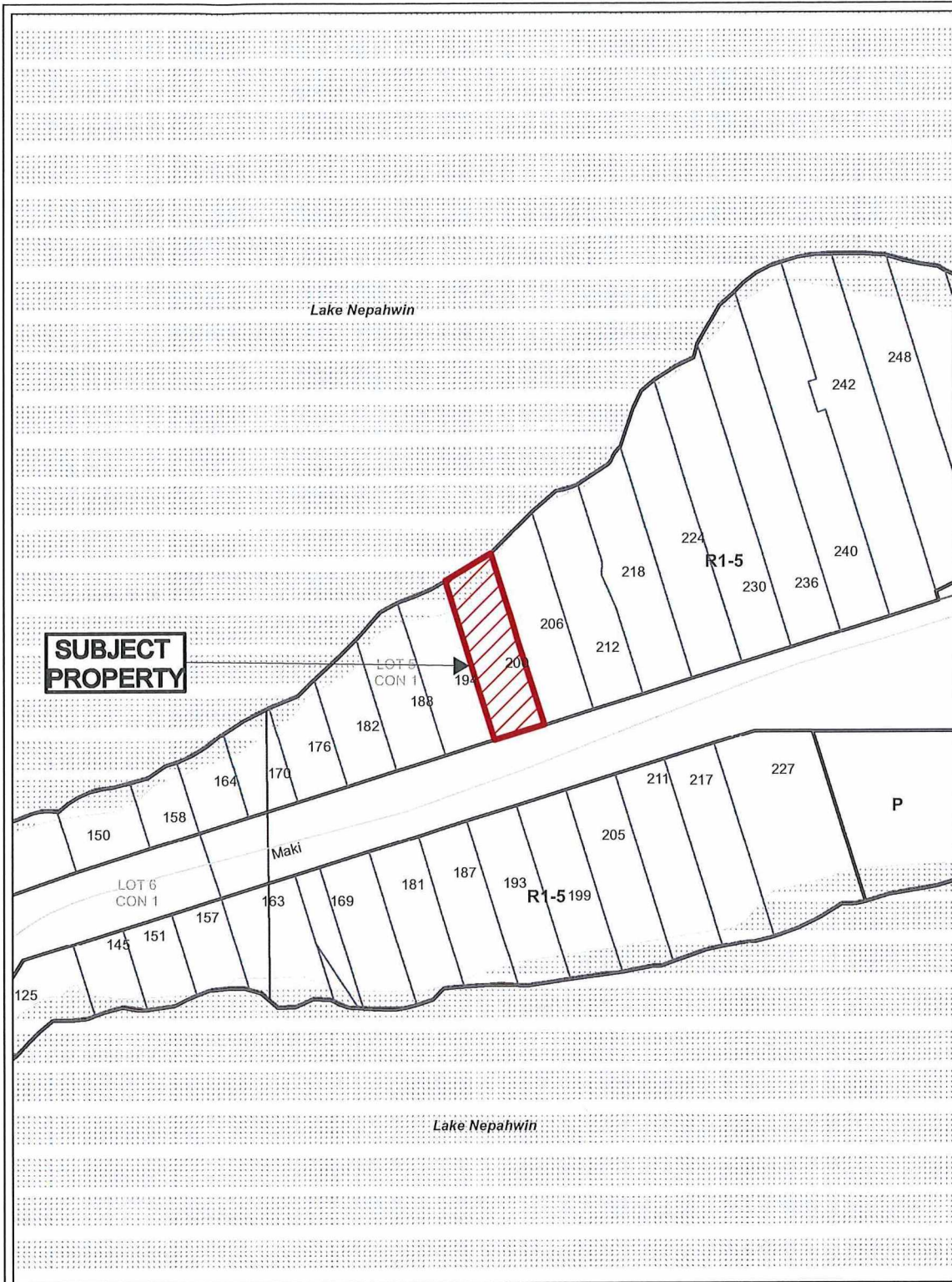
Building Description	Same As Existing	Proposed Ground Floor Area (m2)	Proposed Gross Floor Area (m2)	Proposed Number of Storeys	Proposed Width (m)	Proposed Length (m)	Proposed Height (m)	Proposed Front Yard Setback (m)	Proposed Rear Yard Setback (m)	Proposed Side Yard Setback (m)	Proposed Side Yard Setback Other (m)
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EXISTING BUILDING/STRUCTURE

Building Description	To Be Demolished	Existing Ground Floor Area (m2)	Existing Gross Floor Area (m2)	Existing Number of Storeys	Existing Width (m)	Existing Length (m)	Existing Height (m)	Existing Front Yard Setback (m)	Existing Rear Yard Setback (m)	Existing Side Yard Setback (m)	Existing Side Yard Setback Other (m)
Main dwelling	No	60	120	2	8.1	8.2	6.1	13.9	18.6	6.9	0.3
Detached garage/new accessory dwelling	No	32.2	32.2	1	4.5	7.2	4.6	0	32.8	0.5	10.2

ZONING BY-LAW RELIEF

Variance To	By-law Requirement	Proposed (m)	Difference (m)
Parking requirements - 4.2.10.3 (i)	1 parking space	0 parking spaces	1 parking space
Permit ADU in Front Yard - 4.2.10.3 (e)	Additional dwelling units located in all or part of a building accessory to a main building shall only be permitted within the rear and/or interior side yards	Permit in front yard	n/a
Parking requirements - Part 5, Section 5.5, Table 5.5	1 parking space/unit	0 parking spaces	1 parking space



Application for Minor Variance or Permission



Subject Property being PIN 73594-0052,
 Parcel 8570 SEC SES,
 Lot 28, Plan M-143,
 Part Lot 5, Concession 1,
 Township of McKim,
 200 Maki Avenue, Sudbury,
 City of Greater Sudbury

Sketch 1, NTS

PL-MV-2025-00107

NDCA

Date: 2025 07 22



BY-LAW 2024-1797 COMPLIANCE MATRIX:

4.2.10.3 ADDITIONAL DWELLING UNITS IN ACCESSORY BUILDINGS (BY-LAW 2024-1792)
WHERE AN ADDITIONAL DWELLING UNIT IS LOCATED IN ALL OR PART OF A BUILDING ACCESSORY TO A MAIN BUILDING, THE ADDITIONAL DWELLING UNIT:

- A) SHALL NOT BE PERMITTED TO BE IN THE FORM OF A MOBILE HOME DWELLING IN ALL RESIDENTIAL (R), COMMERCIAL (C), AND 'FD', FUTURE DEVELOPMENT ZONES; COMPLIES - N/A
- B) MAY BE IN THE FORM OF A MOBILE HOME DWELLING IN A RURAL (RU), AGRICULTURAL (A) OR RURAL SHORELINE (RS) ZONES; COMPLIES - N/A
- C) SHALL BE SITED A MINIMUM OF 2.4 M FROM THE MAIN BUILDING; COMPLIES 7.8 m
- D) NOTWITHSTANDING CLAUSE C) ABOVE, THE ADDITIONAL DWELLING UNIT SHALL BE LOCATED NO MORE THAN 30 METRES FROM THE MAIN BUILDING AT ITS CLOSEST, IN THE RURAL (RU), AGRICULTURAL (A) OR RURAL SHORELINE (RS) ZONES; COMPLIES - N/A
- E) SHALL ONLY BE PERMITTED WITHIN THE REAR AND/OR INTERIOR SIDE YARDS; DOES NOT COMPLY - LEGAL NON-COMFORMING
- F) SHALL COMPLY WITH THE SETBACK REQUIREMENTS FOR ACCESSORY BUILDINGS, PER SECTION 4.2 OF THIS BY-LAW; COMPLIES
- G) SHALL NOT EXCEED A MAXIMUM LOT COVERAGE OF 25 PERCENT ON A PARCEL OF URBAN RESIDENTIAL LAND, INCLUSIVE OF THE LOT COVERAGE OF ALL ACCESSORY BUILDINGS AND STRUCTURES ON THE LOT; COMPLIES - SEE ZONING MATRIX
- H) SHALL NOT EXCEED A MAXIMUM HEIGHT OF 8.0 M; COMPLIES - 15' (4.6 m) +/-
- I) SHALL REQUIRE ONE ADDITIONAL PARKING SPACE PER ADDITIONAL DWELLING UNIT THAN WHAT WOULD HAVE BEEN REQUIRED ON THE PROPERTY; DOES NOT COMPLY - NO ONSITE PARKING PROVIDED - OFFSITE PARKING AVAILABLE AT FRONT OF PROPERTY ON ROAD ALLOWANCE
- J) SHALL NOT BE PERMITTED ON A LOT ADJACENT TO A LAKE; COMPLIES
- I. CONTAINING LAKE TROUT WHERE THE LAKE IS OVER THRESHOLD AS LISTED ON APPENDIX C OF THE CITY'S OFFICIAL PLAN; COMPLIES
- II. WHERE A PUBLIC WATER SYSTEM AND A PUBLIC SANITARY SEWER SYSTEM ARE NOT AVAILABLE; AND; COMPLIES
- III. WHERE ANY PORTION OF THE LEACHING BED IS OR WOULD BE WITHIN 300 METRES OF THE SHORELINE OF A LAKE; COMPLIES - N/A
- K) MAY BE PERMITTED ON A LOT; COMPLIES - N/A
- I. ABUTTING A RIVER; OR,
- II. WITH FRONTAGE ON A LAKE WITH PHOSPHORUS CONCERNS, AS DEFINED IN THE OFFICIAL PLAN

SITE PLAN
1:250

ADDRESS: 200 MAKI AVENUE		ZONING BY-LAW: SUDBURY 2.010-100Z		ZONING: R-15 RESIDENTIAL		ZONING MAP
SYMBOL	REGULATION	REQUIREMENT	PROVIDED	COMPLIES ✓ = YES ✗ = NO	NOTES	
	LOT AREA	465 m ²	628.7 m ²	✓		
	BUILDING AREA	NO MIN OR MAX	92.1 m ²	✓		
	LOT COVERAGE	4.0 % MAX (SERVICED)	14.6%	✓		
	NON PERMEABLE SURFACES	NONE STATED	92.1 m ²	✓		
	PERMEABLE REQUIRED	NONE STATED	14.6%	✓		
	LANDSCAPING OF FRONT YARD	50 % OF FRONT YARD	398.8 m ² = 87 %	✓	FRONT YARD AREA = 458.8 m ²	
	LANDSCAPE AREA	NO MIN OR MAX	436.7 m ²	✓		
	LANDSCAPE AREA %	NO MIN OR MAX	69%	✓		
	FRONT YARD SETBACK	6 m	13.9 m HOUSE 0 m ACC. UNIT	✓	LEGAL NON-CONFORMING	
	SIDE YARD SETBACK	1.2 m (FOR ONE STOREY)	0.30 m HOUSE 0.50 m ACC. UNIT 6.90 m HOUSE 10.20 m ACC. UNIT	✓	LEGAL NON-CONFORMING	
	REAR YARD SETBACK	3.0 m	18.60 m HOUSE 32.80 m ACC. UNIT	✓	LEGAL NON-CONFORMING	
	BUILDING HEIGHT	11 m HOUSE 8 m ACC. UNIT	6.15 m HOUSE 4.57 m ACC. UNIT	✓		
	PARKING REQUIREMENTS	TWO SPACE	ZERO	✗	MINOR VARIANCE REQUIRED	
	ACCESSORY DWELLING IN FY	NOT PERMITTED	EXISTING ACCESSORY BLDG IN FY	✗	MINOR VARIANCE REQUIRED	



Box 5000, Station A
200 Brady Street
Sudbury, Ontario P3A 5P3
(705) 671-2489 ext 4376 or 4346
(705) 673-2200 FAX

Record #: PL-MV-2025-00110

APPLICATION SUMMARY

File Date: 07/21/2025

Application Type: Minor Variance

Address(es): 579 Elm Street, Sudbury P3C 1W7

Applicant(s): CATHY DUPUIS

Owner(s): CATHERINE DUPUIS

PLANNING APPLICATION

Are there multiple properties associated with the application?

No

Please describe the additional properties associated with this application

What is the date the current Owner(s) acquired the property?

July 17, 2025

Are you the registered owner or an authorized agent?

Registered Owner

What is the number of dwelling units on the property?

1

What is the number of proposed new dwelling units on the property?

2

What is the number of proposed new buildings/structures on the property?

1

What is the number of existing buildings/structures on the property?

1

If this application is approved, would any existing dwelling units be legalized?

No

How many dwelling units will be legalized?

Is this property located within an area subject to the Greater Sudbury Source Protection Plan?

No

Provide details on how the property is designated in the Source Protection Plan

Current Official Plan designation

Living Area I

Current Official Plan designation (additional)

Current Zoning By-law designation

R2-2

Provide a detailed description of what is being proposed

The existing dwelling of 1408 square feet comprised of 1 basement level and 1 main floor is to be converted into a new duplex dwelling having a basement apartment and a two storey residence. this will be a new built having a total 2304 square feet. this meets the requirements of zoning r2_2.

Provide a detailed reason why the proposal cannot comply with the Zoning By-law

we are reusing the existing foundation floor that does not meet current setbacks on the front, rear and west side. Also the existing deck of 12'x12' is within the rear yard setback.

Is there an eave encroachment?

No

Size of eaves

0

Lot Frontage of the property

11.93

Lot Depth of the property

16.764

Lot Area of the property

200.129

Total width of the public road giving access to the property

20

List all buildings and structures on the property and their respective date of construction

residence. since 1939

Existing use(s) of the subject property and length of time it/they have continued, ex. Residential, Commercial, Institutional, Park, etc.

residential

Is the use remaining the same? If no, please provide the proposed new use

yes

Existing uses of neighbouring properties

residential

Has the property ever been subject of a previous application for minor variance/permission?

No

Do you require zoning relief from Section 4.41 (Waterbodies – Water Frontage, Setbacks and Buffers)?

No

Have you consulted with the Strategic and Environmental Planning department regarding this relief?

Have you consulted with Conservation Sudbury regarding this relief?

WATER SUPPLY AND SEWAGE DISPOSAL

- Municipally owned and operated piped water system
- Municipally owned and operated sanitary sewage system
- Lake
- Pit Privy
- Individual Well
- Communal Well
- Individual Septic System
- Communal Septic System
- Other
- Explain Other

PROPERTY ACCESS

- Provincial highway
- Right-of-way
- Municipal road that is maintained seasonally
- Municipal road that is maintained year-round
- Water

Indicate the parking and docking facilities to be used if via water

Estimate the distance of these facilities from the retained land and nearest public road by water

CONCURRENT APPLICATIONS

Is the property the subject of a current application for Consent?

No

Indicate the application number(s) and status of the application(s)

Is the property the subject of a current application for a Plan of Subdivision?

No

Indicate application number(s) and application status

PROPOSED BUILDING/STRUCTURE

Building Description	Same As Existing	Proposed Ground Floor Area (m2)	Proposed Gross Floor Area (m2)	Proposed Number of Storeys	Proposed Width (m)	Proposed Length (m)	Proposed Height (m)	Proposed Front Yard Setback (m)	Proposed Rear Yard Setback (m)	Proposed Side Yard Setback (m)	Proposed Side Yard Setback Other (m)
residential duplex	Yes	71.349	214.049	2	7.315	9.754	8.839	3	6.401	0.914	1.219

EXISTING BUILDING/STRUCTURE

Building Description	To Be Demolished	Existing Ground Floor Area (m2)	Existing Gross Floor Area (m2)	Existing Number of Storeys	Existing Width (m)	Existing Length (m)	Existing Height (m)	Existing Front Yard Setback (m)	Existing Rear Yard Setback (m)	Existing Side Yard Setback (m)	Existing Side Yard Setback Other (m)
residential	Yes	65.404	130.808	2	7.315	9.754	4.572	3	6.401	0.914	1.219

ZONING BY-LAW RELIEF

Variance To	By-law Requirement	Proposed (m)	Difference (m)
Front yard setback - Table 6.3	15.0m	3.0m	12.0m
Rear yard setback - Table 6.3	7.5m	6.4m	1.1m
Side yard setback - WEST - Table 6.3	1.8m	0.9m	0.9m
Side yard setback - EAST - Table 6.3	1.8m	1.2m	0.6m
Existing lot is only 200.129 sq. m. and the required minimum lot area is 230 sq. m per unit - Table 6.3	230.0 sq. m per unit	100.0 sq. m per unit	130.0 sq. m
Deck-rear yard - Table 4.1	May encroach 3.6 m into the required rear yard but not closer than 3.0 m to the rear lot line.	Encroach 4.5m into required rear yard	0.9
Deck-side yard - Table 4.1	May encroach 1.2 m into the required yard but no closer than 1.2 m to the interior side lot line.	0.914m from interior side lot line	0.286
3.0m wide landscaped area - 4.15.1 (e)	3.0m	0.0m	3.0m

Landscaped Open Space - 4.15.2	A minimum of 50% of all required front and corner side yards shall be maintained as landscaped open space in Low Density Residential One (R1) or Low Density Residential Two (R2) Zones.	No landscaped open space in required front yard	50%
Minimum Parking Space Requirements for a Duplex Dwelling - Table 5.5	1/unit = 2 parking spaces	1 parking space	1
5.4.2 (d) - General Parking Provisions for ground oriented dwellings	Outdoor parking areas are permitted in the required front yard to a maximum of 50% of the width of the minimum lot frontage, or the maximum driveway width established by this by-law, whichever is lesser, in all Residential Zones.	Outdoor Parking Areas to be 100% of the width of the lot frontage	50%
Lot Frontage for Duplex - Table 6.3	15.0m	11.9m	3.1m

New Proposed Duplex - 579 Elm Street, SUDBURY

- Poured Concrete Walls Basement: 71.350 square meters (768 sq. ft.)
 - Wood Frame - FIRST STOREY: 71.350 square meters (768 sq. ft.)
 - Wood Frame - SECOND STOREY: 71.350 square meters (768 sq. ft.)
- TOTAL 214.000 square meters (2304 sq. ft.)

- BASEMENT: APARTMENT (2 BEDROOMS)
- STOREY 1 & 2: APARTMENT (2 BEDROOMS & MASTER BEDROOM c/w ENSUITE)
- THREE - 3 piece washrooms & 1 Halfbath (main floor)

Drawings List

- 1 - Cover Page
- 2 - Site Plan (Existing)
- 3 - Site Plan (Proposed)
- 4 - Deck Details
- 5 - Basement Floor Plan
- 6 - Main Floor Plan
- 7 - Second Floor Plan
- 8 - Side Elevation (East)
- 9 - Side Elevation (West)
- 10 - Front Elevation (Elm)
- 11 - Back Elevation
- 12 - Building Cross-Section

OWNER:
CATHERINE DUPUIS

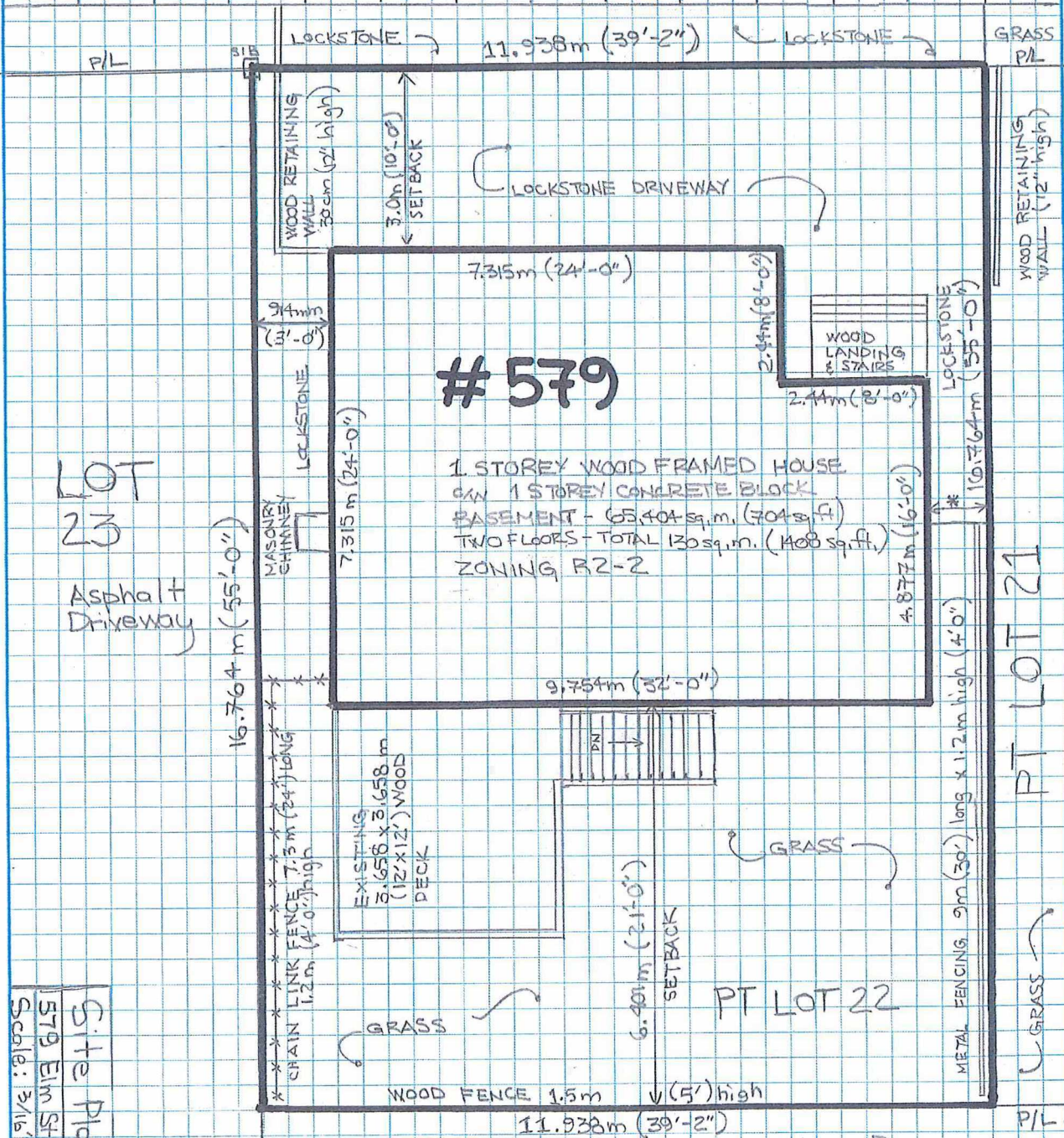
COVER PAGE	
579 Elm Street, SUDBURY	
Scale: NTS	PWA 1

ELM STREET (REGIONAL ROAD # 35)



CONCRETE CURB/GUTTER

1.5m (5') wide sidewalk



Site Plan (Existing)
579 Elm Street Sudbury
Scale: 3/16" = 1'-0"
DWG 2

Reference Plans:
 1- Township of McKim
 Concession 3, Lot 7
 Plan 295, North Part of 22
 2- Greater Sudbury
 CADfile: Elm (M235) 43
 Page no. 4 of 16
 PIN# 73586-9952 LT
 *SETBACK 1.219m (4'-0")

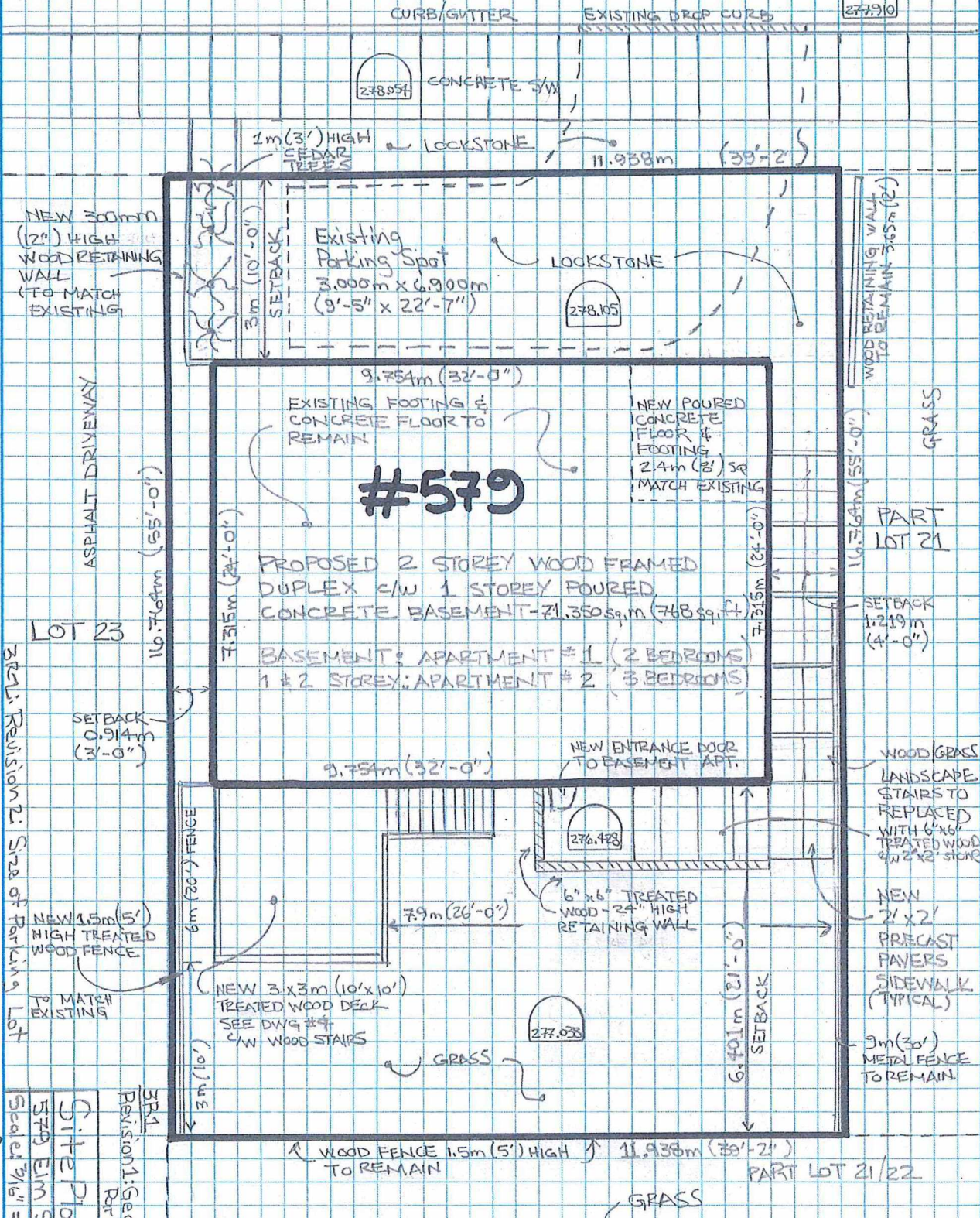
PT LOT 21/22

P/L = PROPERTY LINE

PL-MV-2025-00110 SKETCH 3



ELM STREET



- HOUSE TO BE DEMOLISHED.
 - FOUNDATION (CONCRETE FLOOR) TO REMAIN (TO BE REUSED)
 - EXISTING SETBACKS TO REMAIN
- MINOR VARIANCE (NOTES) - CONVERT TO DUPLEX (R2-2 Zoning)
- NEW SECOND STOREY
 - ONE PARKING SPOT (Rev: 3R1)
 - EXISTING 2 STOREYS - 130 sq.m (1408 sq.ft.)
 - PROPOSED 3 STOREYS - 214 sq.m (2304 sq.ft.)

⊙ DENOTES GEODETIC ELEVATION (METRES)

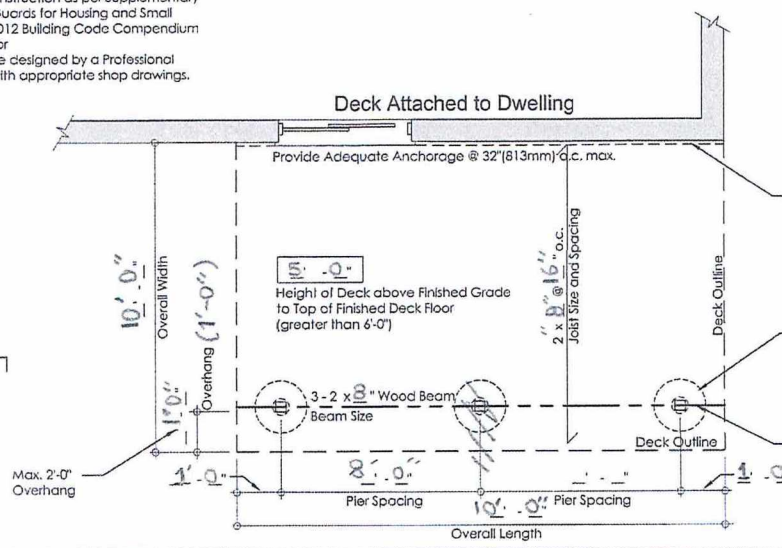
3R1
Revision 1: Geodetic added
Parking Space Revised

Site Plan (Proposed)
579 Elm Street
Scale: 3/16" = 1'-0"

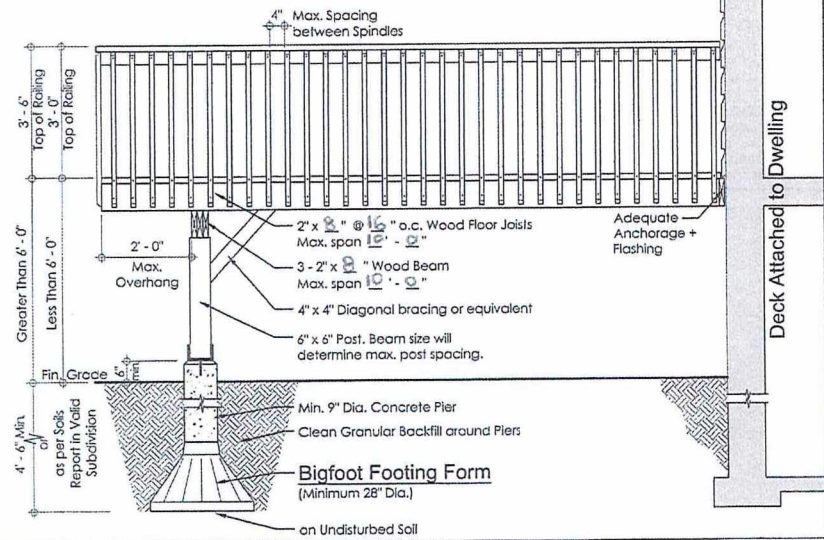
DWG 3R2

Attached Deck Size 10' x 10'

Exterior Guard Construction as per Supplementary Standard S8 - 7 "Guards for Housing and Small Buildings" of the 2012 Building Code Compendium or
Provide alternative designed by a Professional Engineer along with appropriate shop drawings.



- Notes:**
- Ledger board lag bolted to Foundation Wall or Rim Board w/ 1/2" Ø (12.7mm) bolts @ 32"(813mm) o.c. max. into Solid Masonry or Concrete.
 - Deck Floor Joists secured with Joist Hangers to Ledger Board
 - 6" x 6" Pressure Treated Posts Supported on either Concrete Piers with Bigfoot Base, Concrete Pad Footings or Approved Helical Piles
 - Beam Size will be Determined by Distance between Supporting Posts



responsibility to ensure that all the requirements of the Ontario made on these drawings are for information only and do not necessarily construction.



Attached Deck

Deck Plan Scale: nts

DRAWN BY: E.P.
DATE: 14/02/23
AD-1
SHEET

PLUMBING FIXTURE CONNECTION SCHEDULE				
ITEM	COLD WATER	HOT WATER	SANITARY WASTE	VENT PIPING
LAVATORY	1/2" (13mm)	1/2" (13mm)	1 1/2" (38mm)	1 1/2" (38mm)
CLOTHING WASHER	1/2" (13mm)	1/2" (13mm)	2" (50mm)	1 1/2" (38mm)
TOILET	1/2" (13mm)	-	3" (75mm)	1 1/2" (38mm)
SHOWER/TUB	1/2" (13mm)	1/2" (13mm)	2" (50mm)	1 1/2" (38mm)
SINK/DISHWASHER	1/2" (13mm)	1/2" (13mm)	1 1/2" (38mm)	1 1/2" (38mm)



Attached Deck

Detail Scale: nts

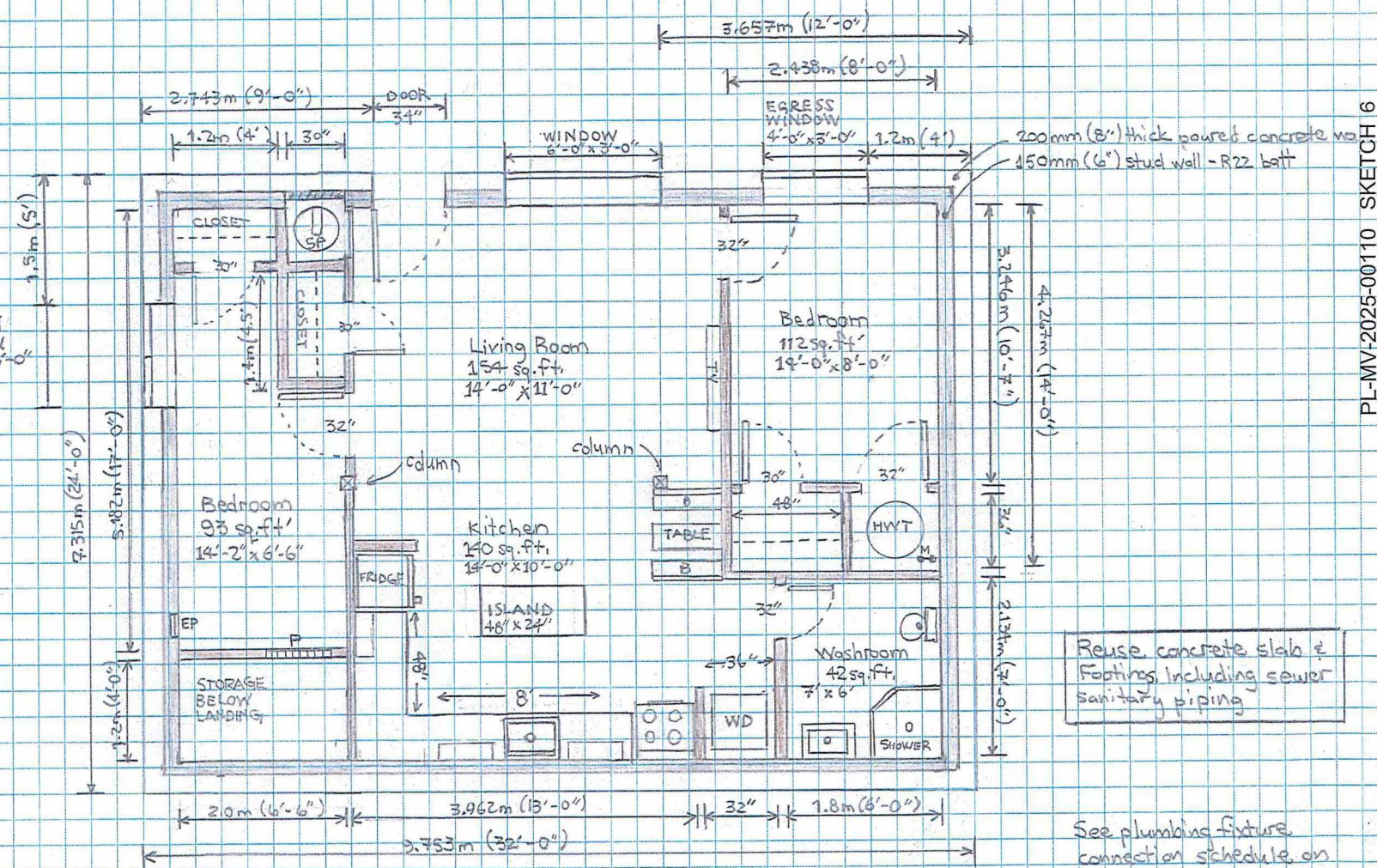
DRAWN BY: E.P.
DATE: 14/02/23
AD-2
SHEET

Deck Details
579 Elm Street, Sudbury
Scale: DWG 4

LEGEND

- ▣ 6"x6" wood column
- ▨ 3"-thick rigid foam
- SP Existing sump pump
- W Existing 3/4" water valve
- HWT New hot water tank
- WD New washer dryer
- B Banquet seating
- P Removal panel access
- EP Electrical Panel

EGRESS WINDOW
4'-0" x 3'-0"



Reuse concrete slab & footings including sewer sanitary piping

See plumbing fixture connection schedule on drawing #4

- All doors height 6'-8"
- All closets are 24" deep unless noted
- All closets doors are 30" wide
- All interiors walls are 2"x4" wood stud frame c/w 1/2" gypsum boards on both sides

ELM STREET

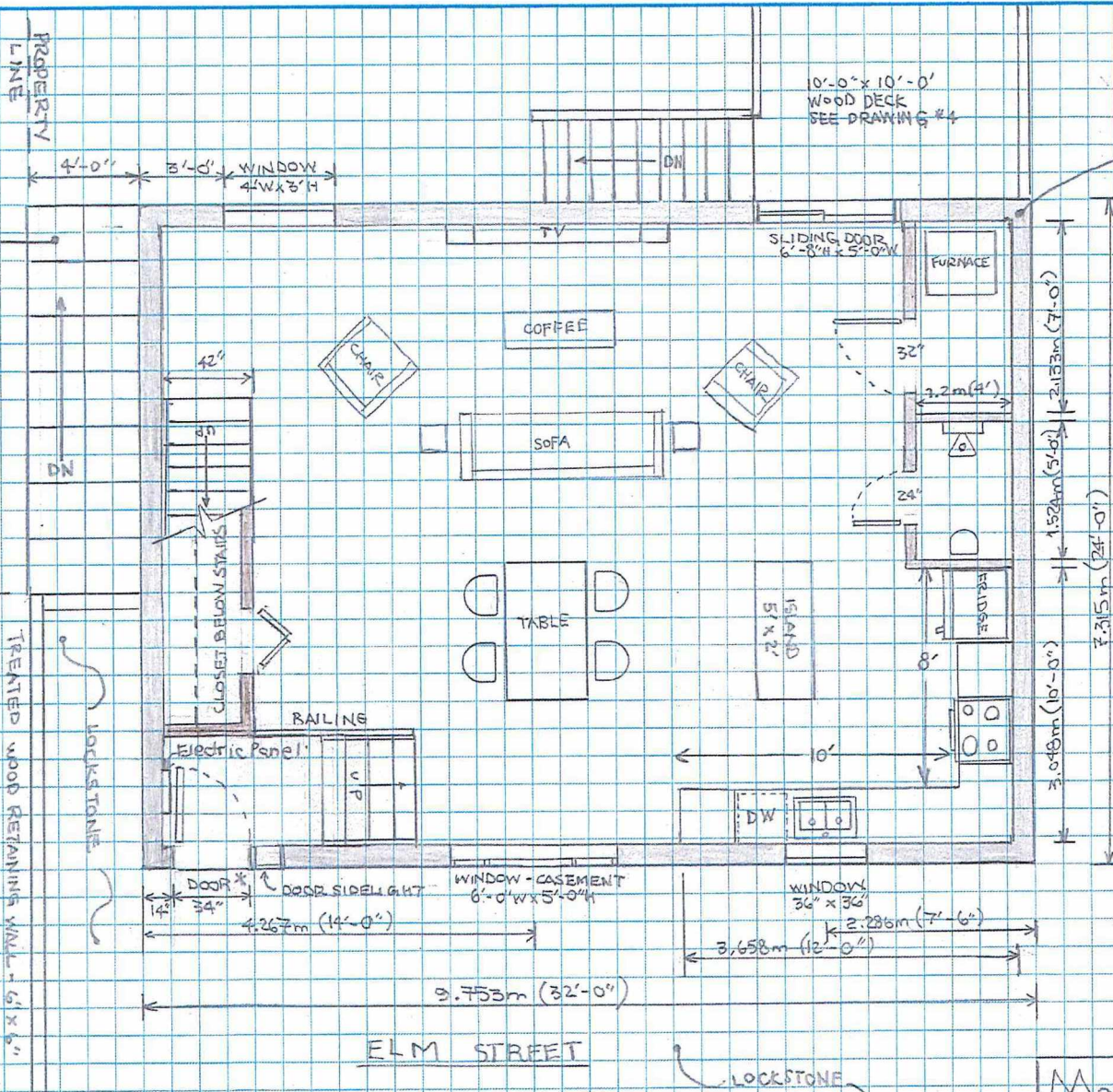
Basement Floor Plan
579 Elm Street, Sudbury
Scale: 1/4" = 1'-0" DWG 5

- All windows to be double panes - low "E" argon gas c/w white vinyl frame/finish

- All new cold and hot water supply/piping to be 1/2" Ø "PEX"

- All new sewer/drainage piping and venting to be "DWV" black piping to match existing

* NEW Entrance Door to be prehung with glass insert & foam insulation, metal painted 6'-8" high x 36" wide



EXTERIOR WALLS
 1/2" drywall (gypsum)
 Two 2"x4" lumber studs
 6 mil poly vapour barrier
 Two R14 "ROXUL" BATT INSUL
 1/2" plywood
 Building wrap (TYPAR)
 Vinyl siding (TYPICAL)

FLOOR
 2"x10" on 16" c/c LUMBER
 5/8" O.S.B SHEATING
 7mm Vinyl plank flooring
 3" high MDF "white baseboards"
 5/8" gypsum "

INTERIOR WALLS
 1/2" drywall (gypsum) on both sides
 2"x4" lumber on 16" c/c

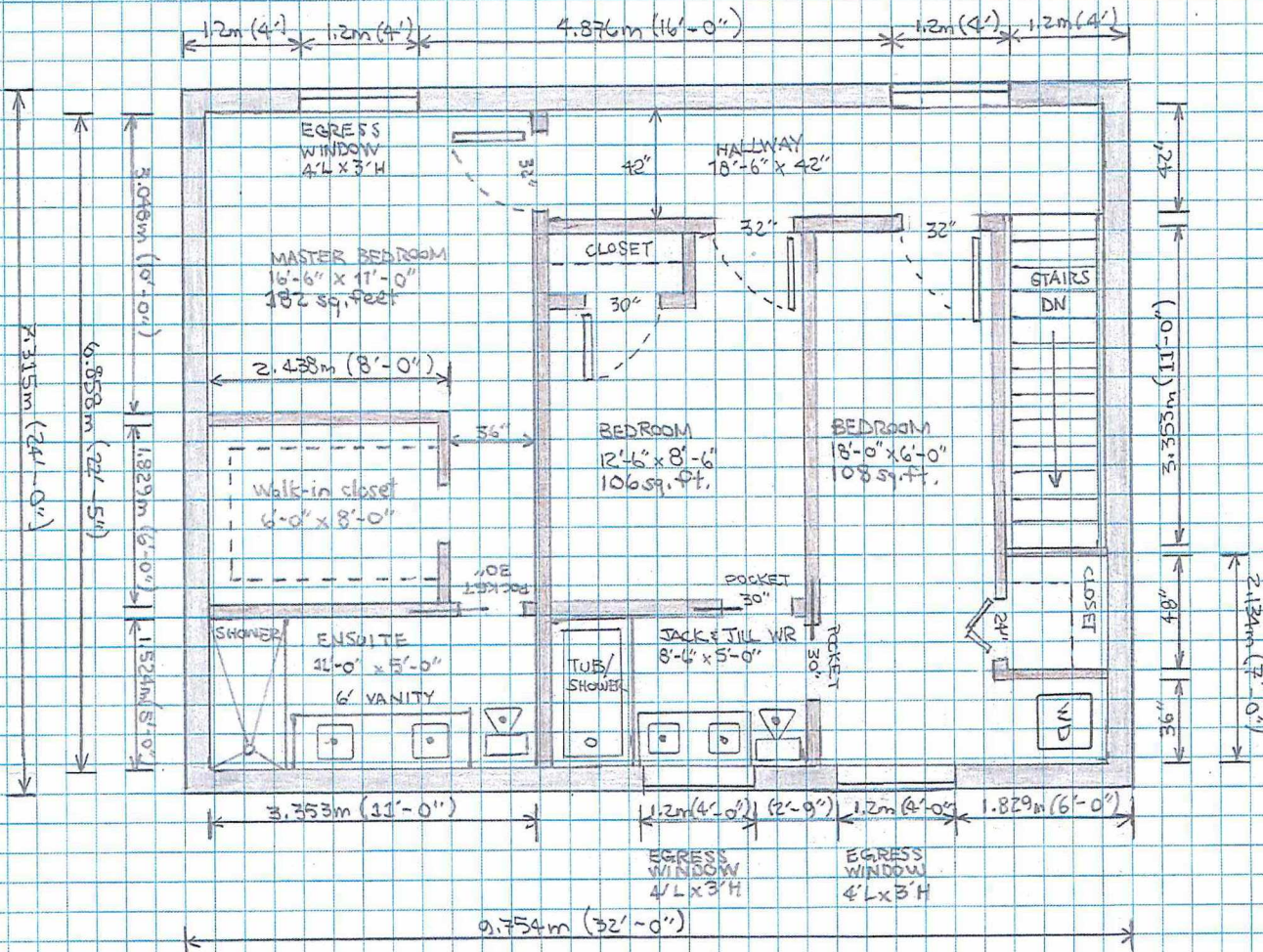
INTERIOR DOORS
 Hollow core - white
 6 panels
 6'-8" height (typical)
 Prehung frame
 3" wide "MDF" trims

See "Plumbing Fixture Connection Schedule" on drawing #4

Main Floor Plan
 579 Elm Street, Sudbury
 Scale: 1/4" = 1'-0" DWG 6

P.L.-MV-2025-00110 SKETCH

ELM STREET

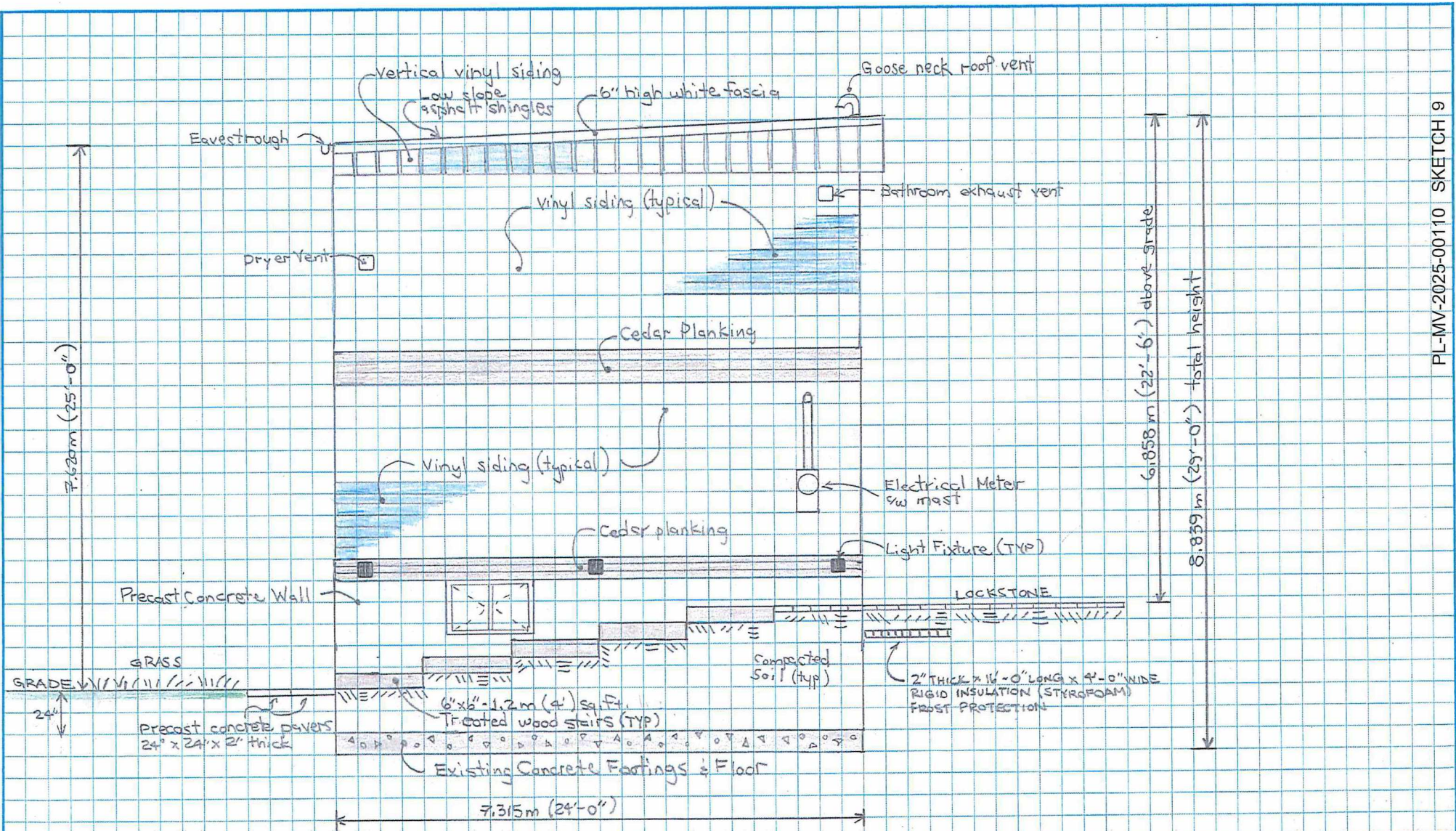


STAIRS
 Rise: 7.500"
 Run: 10.250"
 Width: 39"
 Threads: 15 units

- All windows are vinyl clad egress sliding w/ screen 1.219 m Long x 914mm High 4'-0" long x 3'-0" High w/ "J" siding trim

Four (4) required for this floor level

Second Floor Plan
 579 Elm Street, Sudbury
 Scale 1/4" = 1'-0" Dwg 7



7.620m (25'-0")

6.858m (22'-6") above grade

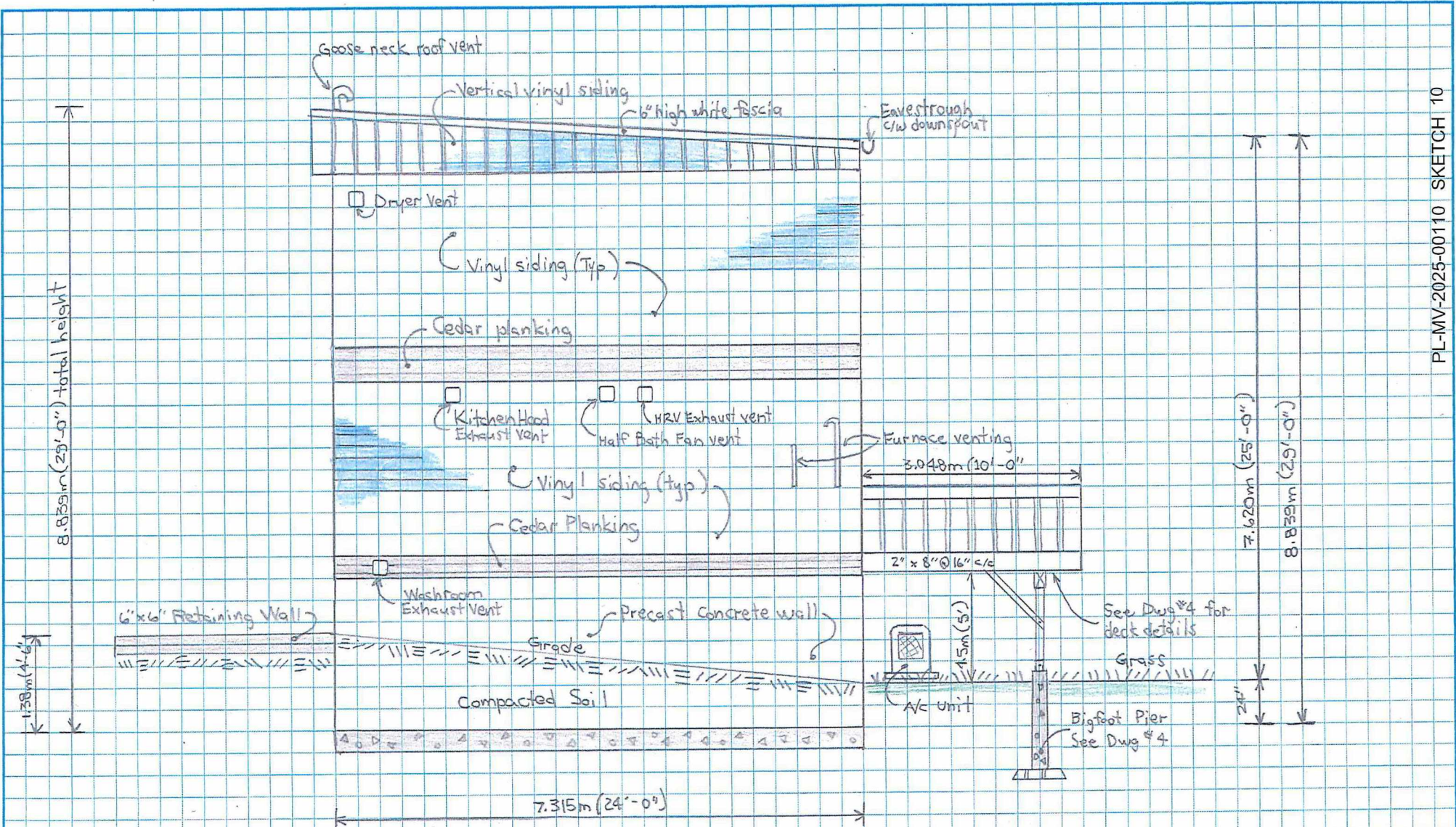
8.839m (29'-0") total height

24"

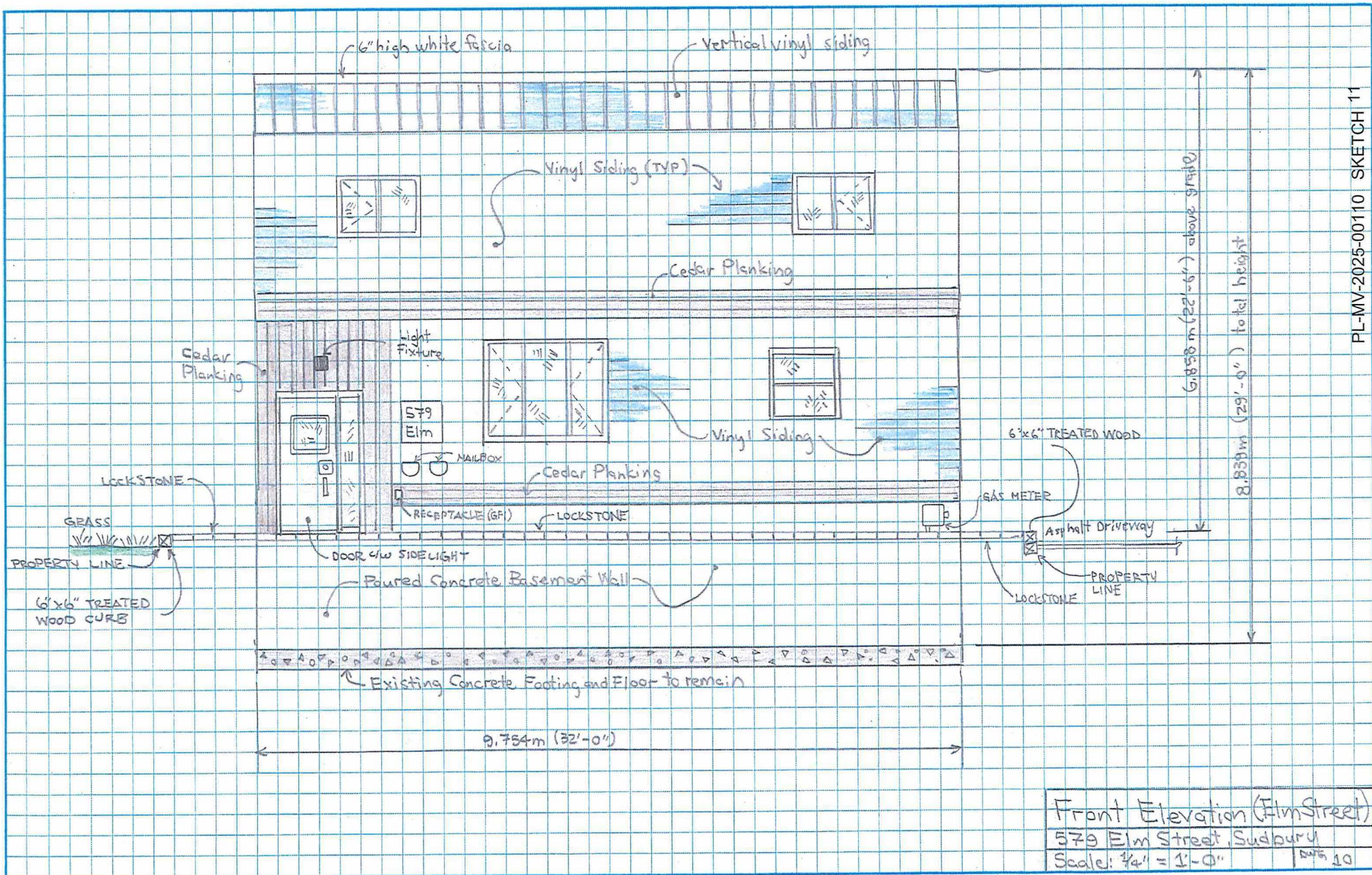
7.315m (24'-0")

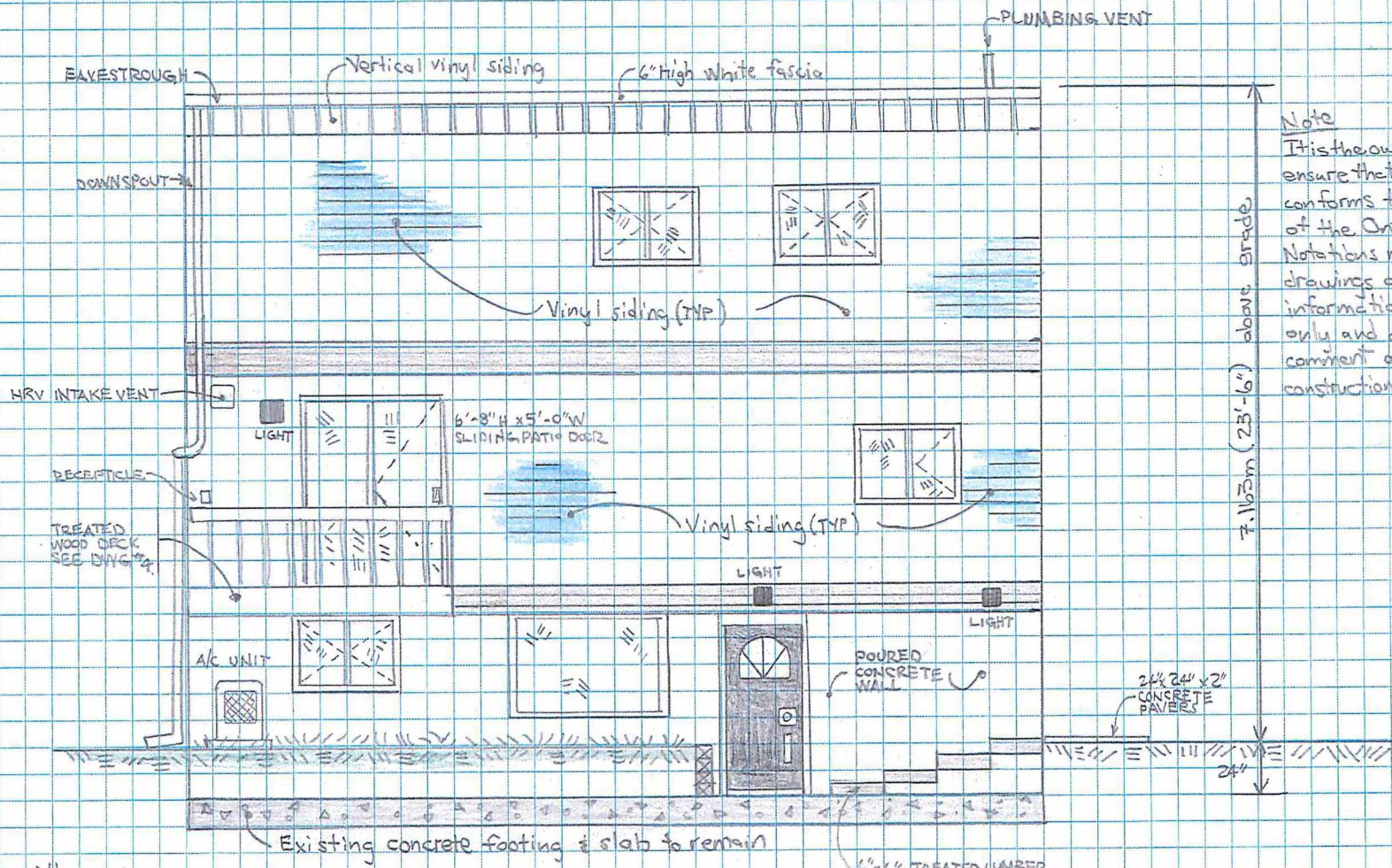
Side Elevation (East)
 579 Elm Street Sudbury
 Scale: 1/4" = 1'-0" DWG 8

PL-MV-2025-00110 SKETCH 9



Side Elevation (West)
 579 Elm Street, Sudbury
 Scale: 1/4" = 1'-0" DWG 9

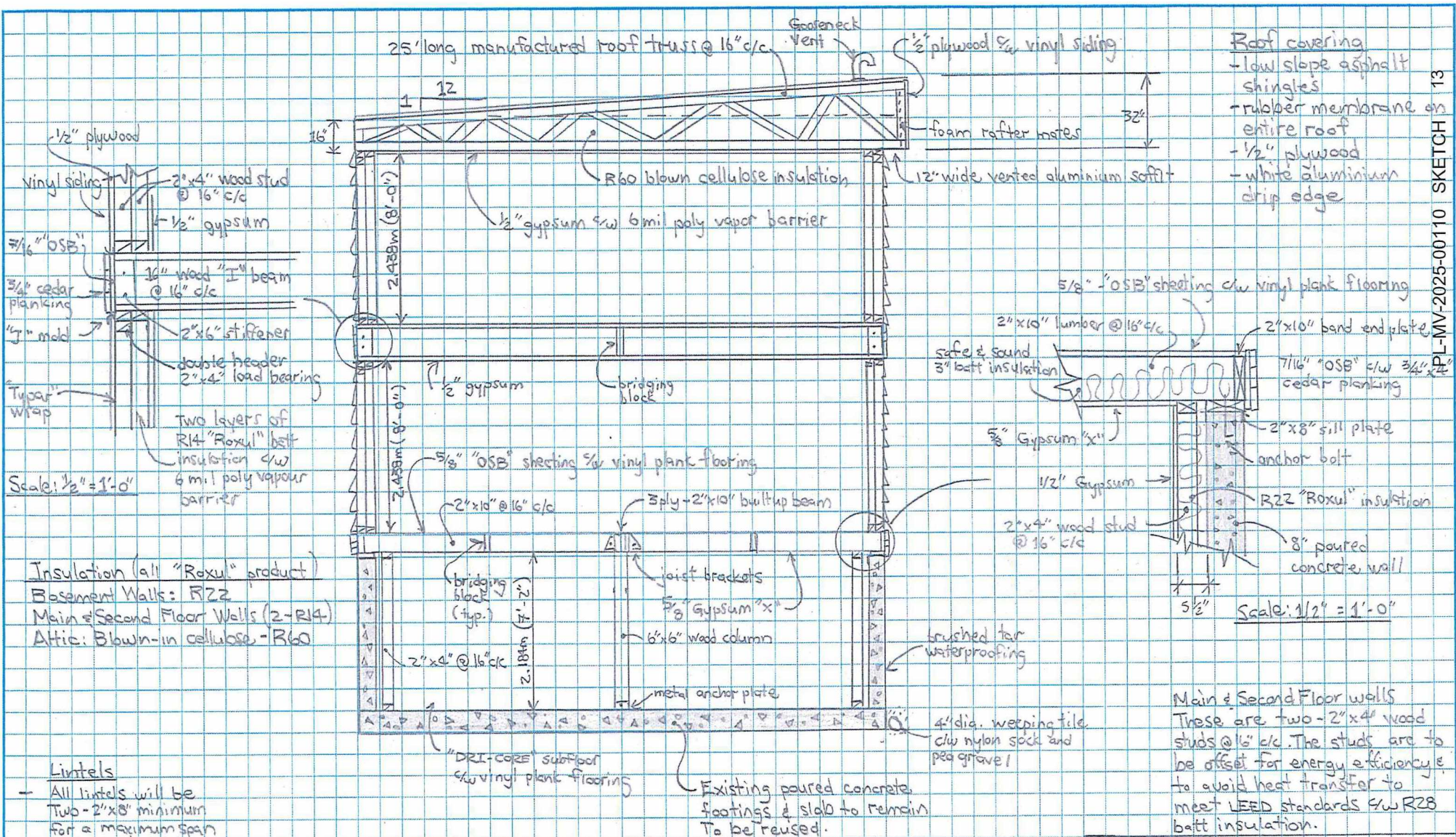




Note
 It is the owner's responsibility to ensure that all construction conforms to the requirements of the Ontario Building Code. Notations made on these drawings are for your information and assistance only and do not necessarily comment on all areas of construction.

Note
 Surface drainage will be directed away from the building and adjoining neighbors

Rear Elevation (Back Yard)
 579 Elm Street, Sudbury
 Scale: 3/4" = 1'-0"



Roof covering
 - low slope asphalt shingles
 - rubber membrane on entire roof
 - 1/2" plywood
 - white aluminium drip edge

1/2" plywood
 Vinyl siding
 2" x 4" wood stud @ 16" c/c
 1/2" gypsum
 5/16" OSB
 3/4" cedar plank
 1" wood
 2" x 6" stiffener
 double header 2" x 4" load bearing
 Typar wrap
 Two layers of R14 "Roxul" batt insulation c/w 6 mil poly vapour barrier
 Scale: 1/2" = 1'-0"

Insulation (all "Roxul" product)
 Basement Walls: R22
 Main & Second Floor Walls (2-R14)
 Attic: Blown-in cellulose - R60

5/8" OSB sheathing c/w vinyl plank flooring
 2" x 10" lumber @ 16" c/c
 safe & sound 3" batt insulation
 2" x 10" band end plate
 7/16" OSB c/w 3/4" x 2" cedar plank
 2" x 8" sill plate
 anchor bolt
 R22 "Roxul" insulation
 8" poured concrete wall
 1/2" Gypsum "x"
 2" x 4" wood stud @ 16" c/c
 Scale: 1/2" = 1'-0"

Lintels
 - All lintels will be Two - 2" x 8" minimum for a maximum span of 1.8 m (5'-11")
 - Two - 2" x 10" span of 3.0 m (10'-0")

Existing poured concrete footings & slab to remain To be reused.

Main & Second Floor walls
 These are two - 2" x 4" wood studs @ 16" c/c. The studs are to be offset for energy efficiency to avoid heat transfer to meet LEED standards c/w R22 batt insulation.

Building Cross-Section
 579 Elm Street, Sudbury
 Scale: 1/4" = 1'-0" Page 12



Box 5000, Station A
200 Brady Street
Sudbury, Ontario P3A 5P3
(705) 671-2489 ext 4376 or 4346
(705) 673-2200 FAX

Record #: PL-MV-2025-00112

APPLICATION SUMMARY

File Date: 07/25/2025

Application Type: Minor Variance

Address(es): 1076 Pine Court, Hanmer P3P 1S1

Applicant(s): BERNARD ROBICHAUD

Owner(s): NICOLE ROBICHAUD AND BERNARD ROBICHAUD

PLANNING APPLICATION

Are there multiple properties associated with the application?

No

Please describe the additional properties associated with this application

What is the date the current Owner(s) acquired the property?

Nov 8 2024

Are you the registered owner or an authorized agent?

Registered Owner

What is the number of dwelling units on the property?

1

What is the number of proposed new dwelling units on the property?

0

What is the number of proposed new buildings/structures on the property?

1

What is the number of existing buildings/structures on the property?

3

If this application is approved, would any existing dwelling units be legalized?

No

How many dwelling units will be legalized?

Is this property located within an area subject to the Greater Sudbury Source Protection Plan?

No

Provide details on how the property is designated in the Source Protection Plan

Current Official Plan designation

Living Area I

Current Official Plan designation (additional)

Current Zoning By-law designation

R1-5

Provide a detailed description of what is being proposed

Proposing to build a detached garage in the front yard.

Provide a detailed reason why the proposal cannot comply with the Zoning By-law

We purchased this property in November 2024 . We were hoping to relocate the driveway and construct a new garage. Since we were told that a minor variance would most likely not be approved to relocate the driveway because of the site triangle of this corner lot, we are applying for a minor variance for the construction of a new garage only. This is what we are proposing:

- that the driveway remain at the existing location
- that construction be permitted for a new garage in the location specified on plot plan submitted to the city
- the location of the new garage would have a man door facing the entrance of the home, facilitating a more accessible point of entry to the home
- the proposed location of the garage is to allocate where the gas line is situated and the overhead hydro line
- the new location of the garage would not obstruct the site triangle of the property
- it would allow ample green space in the front and the side of the property
- it would also allow more usable green space at the side of the house once the existing garage would be removed

Is there an eave encroachment?

Yes

Size of eaves

0.51

Lot Frontage of the property

22.86

Lot Depth of the property

31.06

Lot Area of the property

687.97

Total width of the public road giving access to the property

20

List all buildings and structures on the property and their respective date of construction

Home - 1972

Shed - 2010

Garage 2010

Existing use(s) of the subject property and length of time it/they have continued, ex. Residential, Commercial, Institutional, Park, etc.

Residential

Is the use remaining the same? If no, please provide the proposed new use

yes

Existing uses of neighbouring properties

Residential

Has the property ever been subject of a previous application for minor variance/permission?

Yes

Do you require zoning relief from Section 4.41 (Waterbodies – Water Frontage, Setbacks and Buffers)?

No

Have you consulted with the Strategic and Environmental Planning department regarding this relief?

Have you consulted with Conservation Sudbury regarding this relief?

WATER SUPPLY AND SEWAGE DISPOSAL

Municipally owned and operated piped water system

Municipally owned and operated sanitary sewage system

Lake

Pit Privy

Individual Well

Communal Well

Individual Septic System

Communal Septic System

Other

Explain Other

PROPERTY ACCESS

Provincial highway

Right-of-way

Municipal road that is maintained seasonally

Municipal road that is maintained year-round

Water

Indicate the parking and docking facilities to be used if via water

Estimate the distance of these facilities from the retained land and nearest public road by water

CONCURRENT APPLICATIONS

Is the property the subject of a current application for Consent?

No

Indicate the application number(s) and status of the application(s)

Is the property the subject of a current application for a Plan of Subdivision?

No

Indicate application number(s) and application status

PROPOSED BUILDING/STRUCTURE

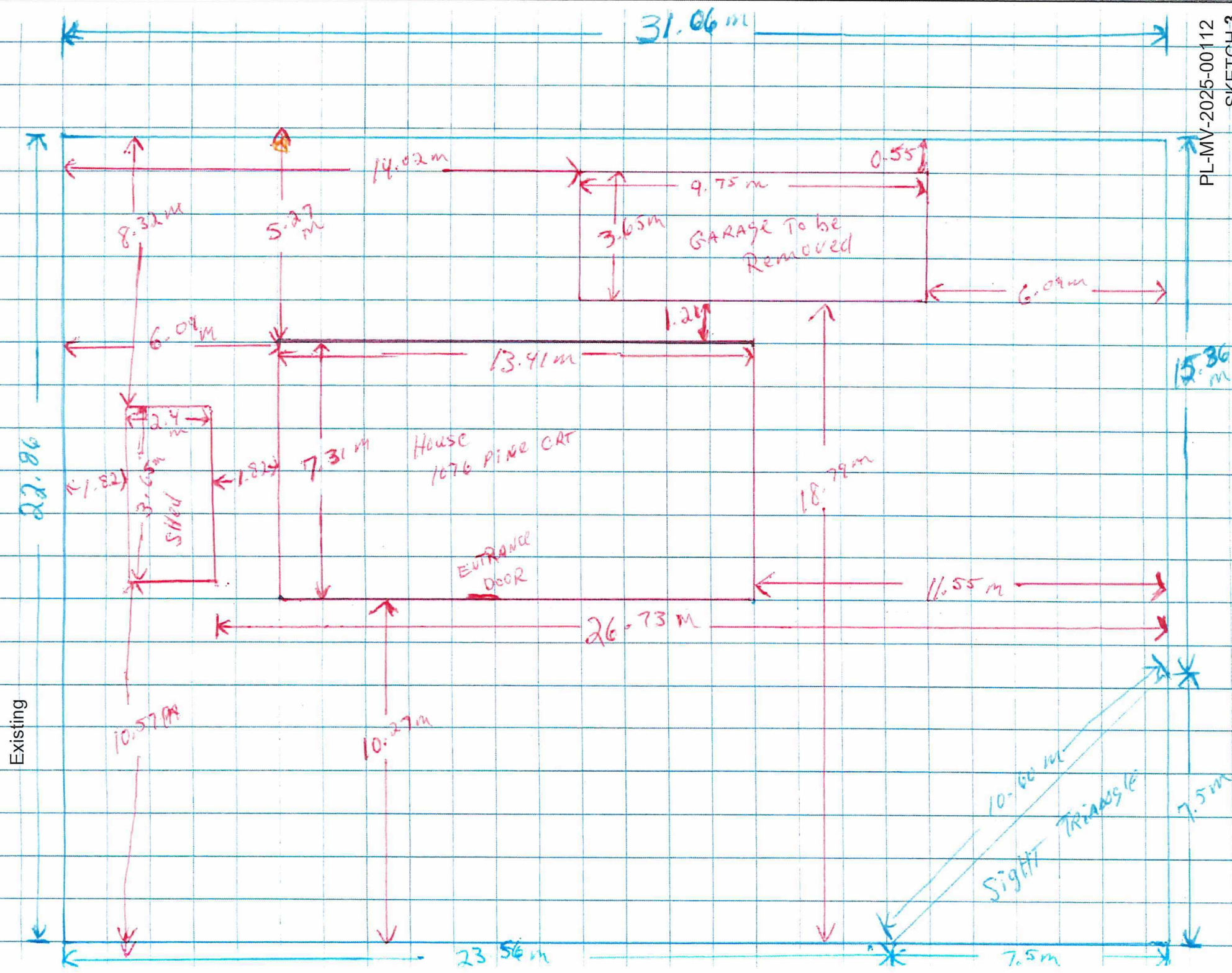
Building Description	Same As Existing	Proposed Ground Floor Area (m2)	Proposed Gross Floor Area (m2)	Proposed Number of Storeys	Proposed Width (m)	Proposed Length (m)	Proposed Height (m)	Proposed Front Yard Setback (m)	Proposed Rear Yard Setback (m)	Proposed Side Yard Setback (m)	Proposed Side Yard Setback Other (m)
Garage	No	53.43	53.43	1	7.31	7.31	4.57	2.4	21.33	10.14	6.61

EXISTING BUILDING/STRUCTURE

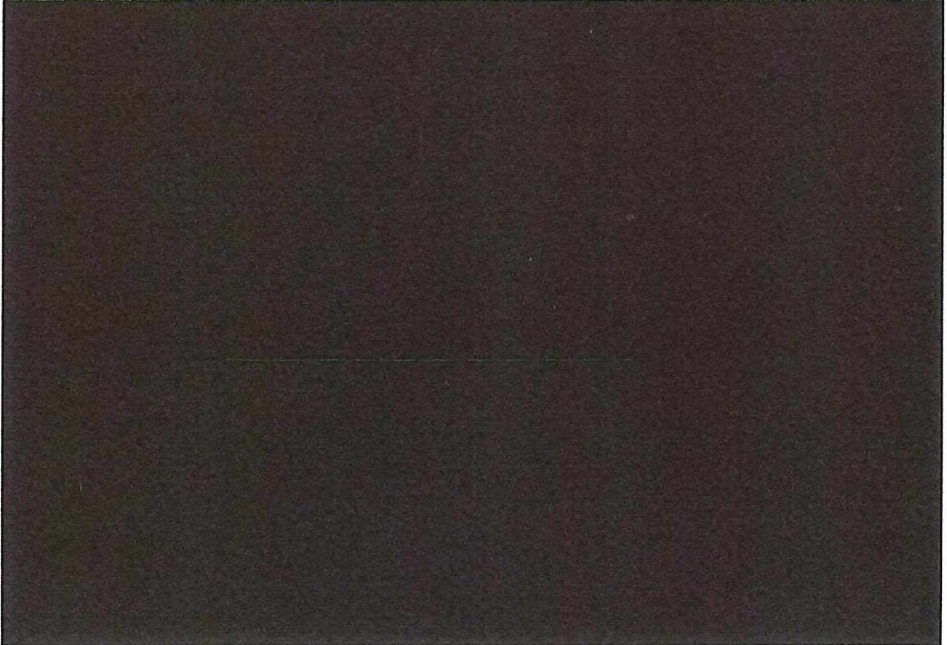
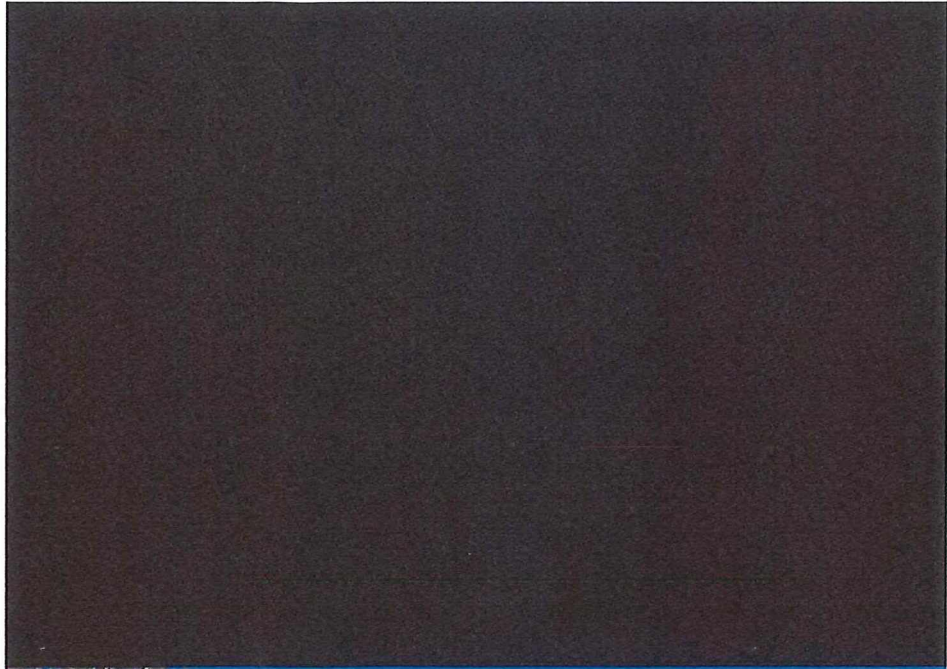
Building Description	To Be Demolished	Existing Ground Floor Area (m2)	Existing Gross Floor Area (m2)	Existing Number of Storeys	Existing Width (m)	Existing Length (m)	Existing Height (m)	Existing Front Yard Setback (m)	Existing Rear Yard Setback (m)	Existing Side Yard Setback (m)	Existing Side Yard Setback Other (m)
Home	No	98.02	98.02	1	7.31	13.41	4.87	11.55	6.09	5.27	10.27
Shed	No	8.76	8.76	1	2.4	3.65	2.13	26.73	1.82	8.32	10.57
Garage	Yes	35.58	35.58	1	3.65	9.75	4.26	6.09	14.02	0.55	18.79

ZONING BY-LAW RELIEF

Variance To	By-law Requirement	Proposed (m)	Difference (m)
Table 6.2 - Garage being in the front yard due to corner lot	6.0m	2.43m	3.57m
Table 4.1 - Garage Eaves	1.2m into 6.0m front yard	4.08m (2.43m setback with eaves encroaching an additional 0.51m)	2.88
4.15.1 (e) - Landscaped Area	3.0m	2.4m	0.6m
4.15.2 - Landscaped Open Space	A minimum of 50% of all required front yard shall be maintained as landscaped open space in Low Density Residential One (R1) Zone.	Minimum 36% of required front yard	14%



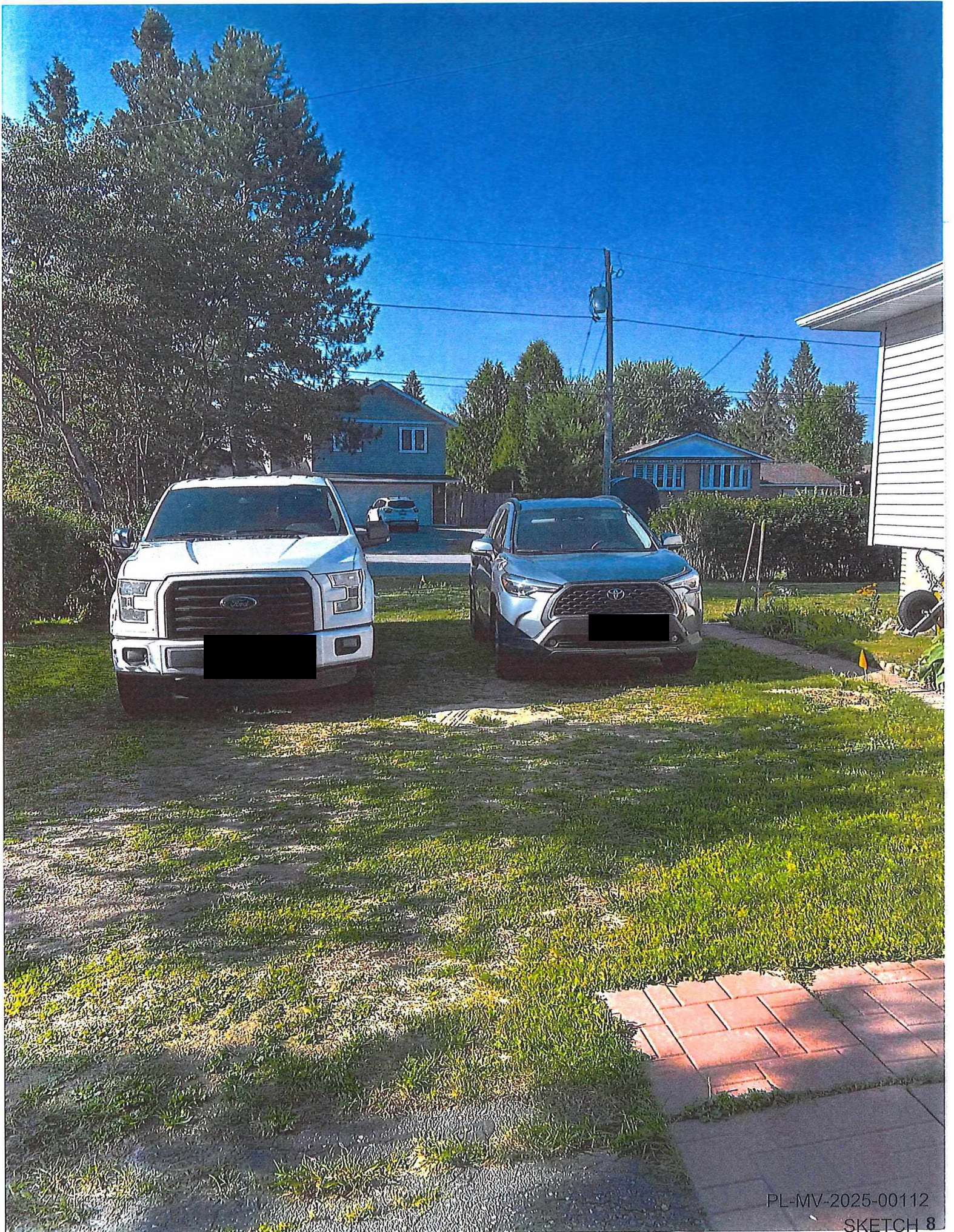
Existing











PL-MV-2025-00112

SKETCH 8



Box 5000, Station A
200 Brady Street
Sudbury, Ontario P3A 5P3
(705) 671-2489 ext 4376 or 4346
(705) 673-2200 FAX

Record #: PL-MV-2025-00114

APPLICATION SUMMARY

File Date: 08/04/2025

Application Type: Minor Variance

Address(es): 60 Marcel Street, Dowling, Ontario P0M 1R0

Applicant(s): KOMRI ENGINEERING

Owner(s): CHRIS MORGAN AND NICOLE MORGAN

PLANNING APPLICATION

Are there multiple properties associated with the application?

No

Please describe the additional properties associated with this application

What is the date the current Owner(s) acquired the property?

June 2015

Are you the registered owner or an authorized agent?

Authorized Agent

What is the number of dwelling units on the property?

1

What is the number of proposed new dwelling units on the property?

1

What is the number of proposed new buildings/structures on the property?

1

What is the number of existing buildings/structures on the property?

3

If this application is approved, would any existing dwelling units be legalized?

No

How many dwelling units will be legalized?

Is this property located within an area subject to the Greater Sudbury Source Protection Plan?

No

Provide details on how the property is designated in the Source Protection Plan

Current Official Plan designation

Living Area I

Current Official Plan designation (additional)

Current Zoning By-law designation

R1-5

Provide a detailed description of what is being proposed

The application requests relief to permit an additional dwelling unit (ADU) within a portion of the building located in the exterior side yard, as well as relief from the maximum combined lot coverage for accessory structures, currently limited to 10%.

The proposed use of the attached dwelling unit within the garage is intended to serve as an in-law suite for the owner's widowed mother, providing a safe, accessible, and supportive living space in close proximity to family. This ensures the occupant's comfort and security while allowing the family to provide care and maintain close contact.

Provide a detailed reason why the proposal cannot comply with the Zoning By-law

The purpose of the proposed garage will provide safe, secure indoor parking for multiple vehicles, particularly during winter months when outdoor parking may be limited or hazardous. It also offers storage for seasonal equipment, such as lawn care tools, bicycles, and snowblowers, helping to reduce outdoor clutter and maintain an organized property. In addition, the garage includes a modest workshop area to support general household maintenance and DIY projects, avoiding the need for off-site storage or workspace.

Zoning Compliance and Requested Relief

Lot Coverage

The increased lot coverage is necessary to accommodate the building's intended use and layout. Given site limitations including setbacks, grading, and access the proposed coverage provides the most effective and functional footprint without overdevelopment.

Building Location / Exterior Yard

Relief is requested for the building's location within the exterior yard. The lot's shape and orientation require thoughtful placement of the building to ensure proper access and integration with the streetscape. The proposed location enhances street presence while still maintaining sufficient setbacks for landscaping and circulation.

Is there an eave encroachment?

No

Size of eaves

Lot Frontage of the property

39.7

Lot Depth of the property

39.7

Lot Area of the property

1560

Total width of the public road giving access to the property

20.0

List all buildings and structures on the property and their respective date of construction

rear yard shed - october 2015

dwelling - 1989

front yard shed - before 2009

Existing use(s) of the subject property and length of time it/they have continued, ex. Residential, Commercial, Institutional, Park, etc.

Residential

Is the use remaining the same? If no, please provide the proposed new use

Remaining the same

Existing uses of neighbouring properties

Residential

Has the property ever been subject of a previous application for minor variance/permission?

No

Do you require zoning relief from Section 4.41 (Waterbodies – Water Frontage, Setbacks and Buffers)?

No

Have you consulted with the Strategic and Environmental Planning department regarding this relief?

Have you consulted with Conservation Sudbury regarding this relief?

WATER SUPPLY AND SEWAGE DISPOSAL

- Municipally owned and operated piped water system
- Municipally owned and operated sanitary sewage system
- Lake
- Pit Privy
- Individual Well
- Communal Well
- Individual Septic System
- Communal Septic System
- Other
- Explain Other

PROPERTY ACCESS

- Provincial highway
- Right-of-way
- Municipal road that is maintained seasonally
- Municipal road that is maintained year-round
- Water

Indicate the parking and docking facilities to be used if via water

Estimate the distance of these facilities from the retained land and nearest public road by water

CONCURRENT APPLICATIONS

Is the property the subject of a current application for Consent?
No

Indicate the application number(s) and status of the application(s)

Is the property the subject of a current application for a Plan of Subdivision?
No

Indicate application number(s) and application status

PROPOSED BUILDING/STRUCTURE

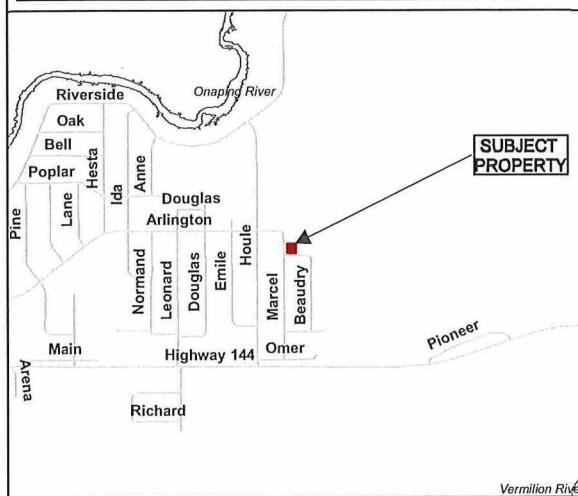
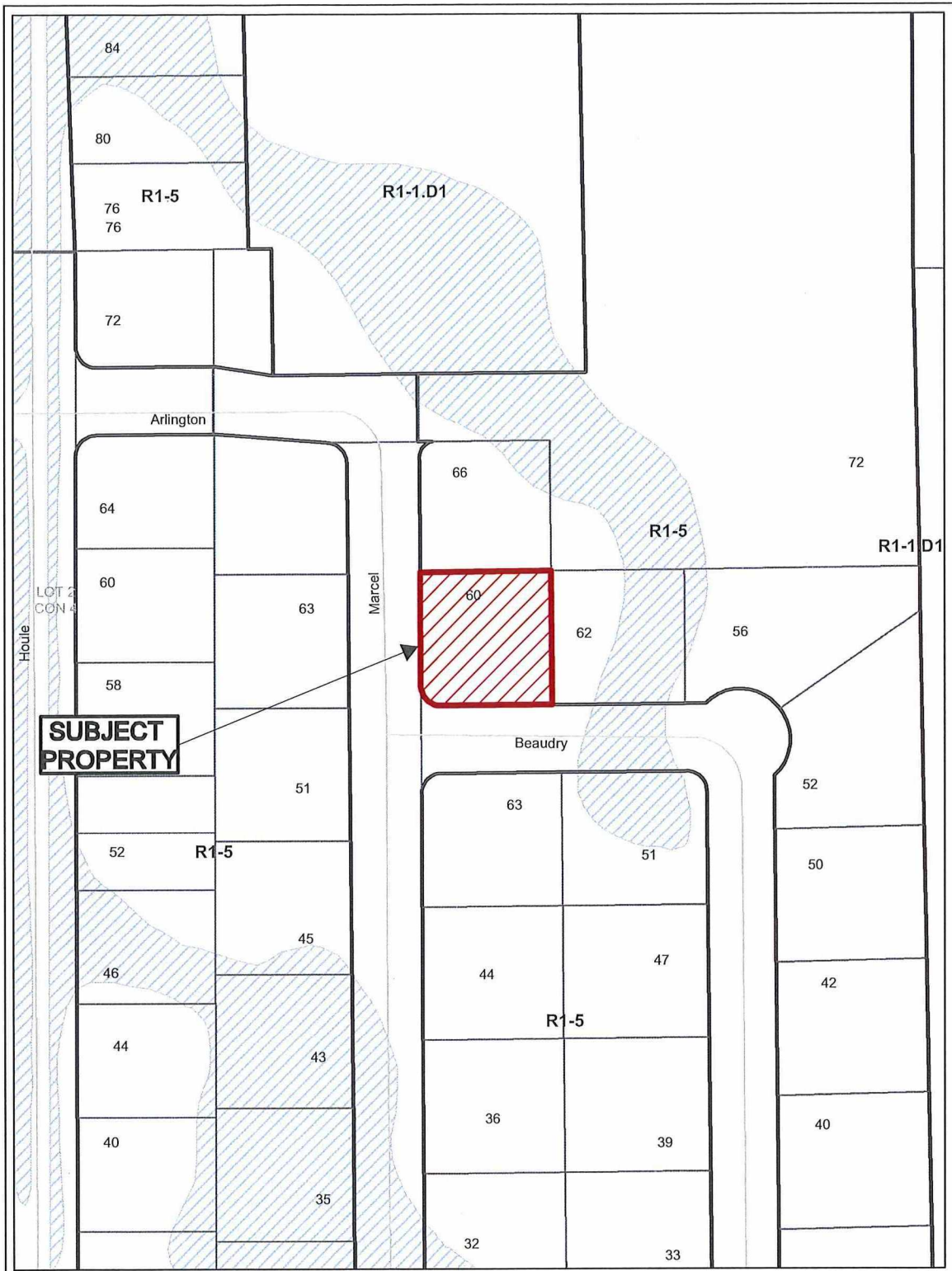
Building Description	Same As Existing	Proposed Ground Floor Area (m ²)	Proposed Gross Floor Area (m ²)	Proposed Number of Storeys	Proposed Width (m)	Proposed Length (m)	Proposed Height (m)	Proposed Front Yard Setback (m)	Proposed Rear Yard Setback (m)	Proposed Side Yard Setback (m)	Proposed Side Yard Setback Other (m)
Garage + Additional Dwelling Unit	No	254.1	320.9	1	17.4	14.6	6.8	14.6	10.4	4.5	17.7

EXISTING BUILDING/STRUCTURE

Building Description	To Be Demolished	Existing Ground Floor Area (m ²)	Existing Gross Floor Area (m ²)	Existing Number of Storeys	Existing Width (m)	Existing Length (m)	Existing Height (m)	Existing Front Yard Setback (m)	Existing Rear Yard Setback (m)	Existing Side Yard Setback (m)	Existing Side Yard Setback Other (m)
Dwelling	No	110.0	220.0	1	10.2	13.2	6	12.0	14.4	25.5	4.0
accessory structure	No	10.0	10.0	1	3.4	3.1	4	34.5	2.1	14.7	21.6
accessory structure	No	10.0	10.0	1	3.4	3.1	4	34.5	2.1	18.7	17.6

ZONING BY-LAW RELIEF

Variance To	By-law Requirement	Proposed (m)	Difference (m)
Accessory Lot Coverage - Subsection 4.2.3	10%	18%	8%
Locate ADU in exterior yard - Subsection 4.2.10.3 (e)	Not permitted. ADU permitted in interior side yard and/or rear yard only	Permit in Exterior Yard	n/a



**Application for Minor
Variance or Permission** 

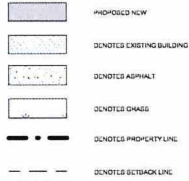
Subject Property being PIN 73352-0720,
Parcel 25660 SEC SWS SRO,
Lot 17, Plan M-1041,
Part Lot 2, Concession 4,
Township of Dowling,
60 Marcel Street, Dowling,
City of Greater Sudbury

Sketch 1, NTS PL-MV-2025-00114
NDCA Date: 2025 08 15

GENERAL NOTES:

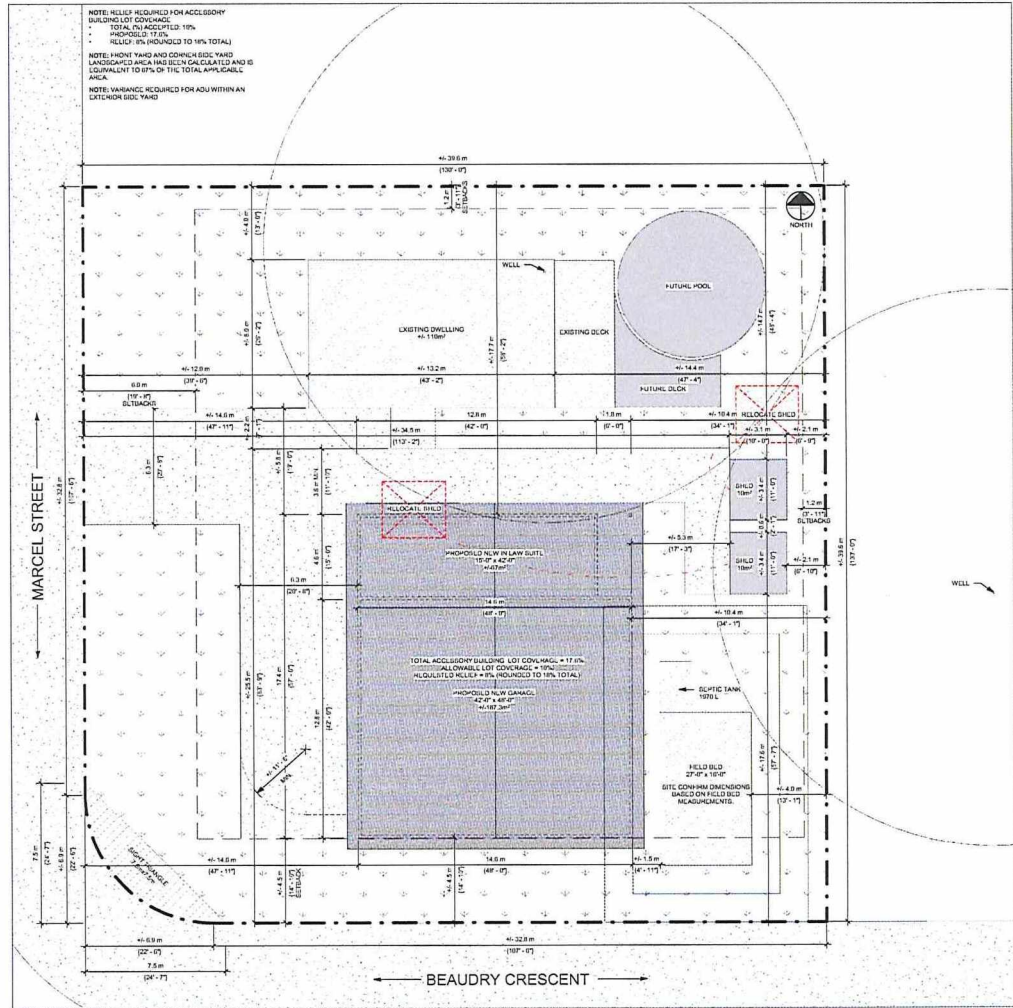
- EXISTING REFER TO DRAWING PREPARED BY SURVEYOR FOR GRADING INFORMATION.
- CONTRACTOR TO LOCATE ALL UNDERGROUND SERVICES PRIOR TO COMMENCEMENT OF WORK.

SITE PLAN LEGEND



DIMENSIONS ON SITE PLAN TO BE FIELD VERIFIED BY HOME OWNER. CONTRACTOR OR SITE SURVEYOR. ANY DISCREPANCIES SHALL BE REPORTED TO NORTH ENGINEERING.

ZONING	
TOTAL PROPERTY AREA	= 10,000 sq ft
PROPOSED NEW BUILDING	= 254.3 sq ft
EXISTING DWELLING	= 1,300 sq ft
NEW TOTAL LOT COVERAGE	= 24.43%
MAXIMUM LOT COVERAGE	
MINIMUM FRONT LANDSCAPE AREA	= 50%
MAXIMUM HEIGHT	= 11m
MINIMUM LOT FRONTAGE	= 7.5m
MINIMUM FRONT YARD	= 3.0m
MINIMUM REAR YARD	= 3.0m
MINIMUM SIDE YARD	= 1.2m
MINIMUM CORNER SIDE YARD	= 4.5m
ACCESSORY STRUCTURE ALLOWANCES	
MAXIMUM HEIGHT	= 6m
MAX LOT COVERAGE	= 10%
PROPOSED NEW BUILDINGS	= 17.6%
MINIMUM FRONT YARD	= 3.0m
MINIMUM REAR YARD	= 3.0m
MINIMUM SIDE YARD	= 1.2m
MINIMUM CORNER SIDE YARD	= 4.5m



SITE PLAN
 1" = 10'-0"

GENERAL NOTES:
 1. CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO COMMENCEMENT OF WORK.
 2. CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO COMMENCEMENT OF WORK.
 3. CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO COMMENCEMENT OF WORK.
 4. CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO COMMENCEMENT OF WORK.

2023/04/11
 NORTH ENGINEERING

116 DUNDAS STREET, GURURU, ONT. M3B 1G1
 416-929-8999 ext 101

KOMRI ENGINEERING
 116 DUNDAS STREET, GURURU, ONT. M3B 1G1
 416-929-8999 ext 101

CHRIS BURGON
 2023/04/11
 PROJECT NO. 2023/04/11

DRAWN: CB
 CHECKED: DP/IKO
 SCALE: As Indicated
 SHEET: A01

ISSUED FOR PERMIT - AUGUST 19th, 2025

PL-MV-2025-00114
 SKETCH 2



Box 5000, Station A
200 Brady Street
Sudbury, Ontario P3A 5P3
(705) 671-2489 ext 4376 or 4346
(705) 673-2200 FAX

Record #: PL-MV-2025-00118

APPLICATION SUMMARY

File Date: 08/12/2025

Application Type: Minor Variance

Address(es): 4176 Municipal Road 35 Road, 4176 Municipal Road 35, Chelmsford P0M1L0

Applicant(s): TULLOCH

Owner(s): JOSEE ROBERT AND JAMES ROBERT

PLANNING APPLICATION

Are there multiple properties associated with the application?

Yes

Please describe the additional properties associated with this application

PINs 73347-1438 & 73347-1440

What is the date the current Owner(s) acquired the property?

September 21, 2023

Are you the registered owner or an authorized agent?

Authorized Agent

What is the number of dwelling units on the property?

0

What is the number of proposed new dwelling units on the property?

1

What is the number of proposed new buildings/structures on the property?

2

What is the number of existing buildings/structures on the property?

0

If this application is approved, would any existing dwelling units be legalized?

No

How many dwelling units will be legalized?

Is this property located within an area subject to the Greater Sudbury Source Protection Plan?

No

Provide details on how the property is designated in the Source Protection Plan

Current Official Plan designation

Rural

Current Official Plan designation (additional)

Current Zoning By-law designation

RU

Provide a detailed description of what is being proposed

See attached cover letter

Provide a detailed reason why the proposal cannot comply with the Zoning By-law

See attached cover letter

Is there an eave encroachment?

No

Size of eaves

Lot Frontage of the property

200

Lot Depth of the property

1000

Lot Area of the property

538232

Total width of the public road giving access to the property

61

List all buildings and structures on the property and their respective date of construction

N/A

Existing use(s) of the subject property and length of time it/they have continued, ex. Residential, Commercial, Institutional, Park, etc.

Vacant

Is the use remaining the same? If no, please provide the proposed new use

Proposed residential use

Existing uses of neighbouring properties

Residential

Has the property ever been subject of a previous application for minor variance/permission?

No

Do you require zoning relief from Section 4.41 (Waterbodies – Water Frontage, Setbacks and Buffers)?

No

Have you consulted with the Strategic and Environmental Planning department regarding this relief?

Have you consulted with Conservation Sudbury regarding this relief?

WATER SUPPLY AND SEWAGE DISPOSAL

- Municipally owned and operated piped water system
- Municipally owned and operated sanitary sewage system
- Lake
- Pit Privy
- Individual Well
- Communal Well
- Individual Septic System
- Communal Septic System
- Other
- Explain Other

PROPERTY ACCESS

- Provincial highway
- Right-of-way
- Municipal road that is maintained seasonally
- Municipal road that is maintained year-round
- Water

Indicate the parking and docking facilities to be used if via water

Estimate the distance of these facilities from the retained land and nearest public road by water

CONCURRENT APPLICATIONS

Is the property the subject of a current application for Consent?

No

Indicate the application number(s) and status of the application(s)

Is the property the subject of a current application for a Plan of Subdivision?

No

Indicate application number(s) and application status

PROPOSED BUILDING/STRUCTURE

Building Description	Same As Existing	Proposed Ground Floor Area (m2)	Proposed Gross Floor Area (m2)	Proposed Number of Storeys	Proposed Width (m)	Proposed Length (m)	Proposed Height (m)	Proposed Front Yard Setback (m)	Proposed Rear Yard Setback (m)	Proposed Side Yard Setback (m)	Proposed Side Yard Setback Other (m)
Single detached dwelling	No	347	694	2	11.5	26	7	163.89	709.27	33.8	350.31
Detached garage	No	557.48	557.48	1	18.29	30.48	8	201.35	690.61	80.24	311.12

EXISTING BUILDING/STRUCTURE

Building Description	To Be Demolished	Existing Ground Floor Area (m2)	Existing Gross Floor Area (m2)	Existing Number of Storeys	Existing Width (m)	Existing Length (m)	Existing Height (m)	Existing Front Yard Setback (m)	Existing Rear Yard Setback (m)	Existing Side Yard Setback (m)	Existing Side Yard Setback Other (m)
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ZONING BY-LAW RELIEF

Variance To	By-law Requirement	Proposed (m)	Difference (m)
Maximum height of accessory structure (detached garage) (Section 4.2.4)	6.5 metres	8 metres	1.5 metres



Planners | Surveyors | Biologists | Engineers

August 12, 2025
File No. 25-2535

Development Approvals – Planning Services
Tom Davies Square – City of Greater Sudbury
200 Brady Street
Sudbury, ON
P3A 5P3

Re: Minor Variance – 4176 Municipal Road 35, Chelmsford
PINs 73347-1438 & 73347-1440

Dear Planning Services,

TULLOCH is retained by the owner(s) of 4176 Municipal Road 35 (being PINs 73347-1438 & 73347-1440) in Chelmsford to facilitate the submission of a minor variance application that seeks relief for the height of a proposed detached garage. The garage is proposed to have a height of ± 8 metres, whereas 6.5 metres is otherwise required under **Section 4.2.4** of the zoning by-law.

The property owner began the building permit process for the development envisioned on the submitted Site Plan. Through this process, the owner was advised that the above noted relief is required. They were also advised that the subject property contains areas regulated by Conservation Sudbury and are currently working with Conservation Sudbury to prepare and submit a Section 28 Permit for any work proposed in their regulated area.

The purpose of the proposed detached garage is for general storage, along with the storage of a personal semi-truck cab. Additional height for the garage is needed to accommodate the semi-truck cab. The proposed variance is minor and desirable for the appropriate development of the land given the following:

- A detached garage is permitted accessory to the proposed single detached dwelling in the RU zone to a maximum lot coverage of 10%, whereas the proposed detached garage would contain a lot coverage of $\pm 0.11\%$;
- Existing mature vegetation throughout the property would act as a natural buffer between the garage and the surrounding area;

- The main use of the property is proposed to be a single detached dwelling with a height of ± 7 metres. Although the garage would exceed this dwelling in height, it would be subordinate in size and location on site; and
- The RU zone permits a maximum height of 11 metres for residential uses, whereas the proposed minor variance would have the effect of restricting the maximum height of the garage to 8 metres. Therefore, the garage would also be subordinate with respect to maximum height permissions in the zoning by-law.

Please find attached the following information in support of a complete application:

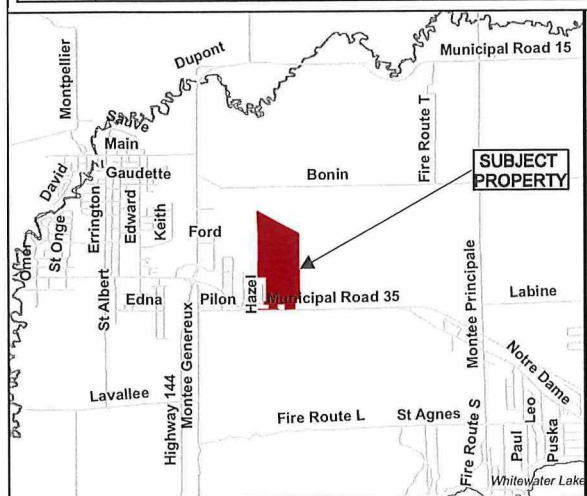
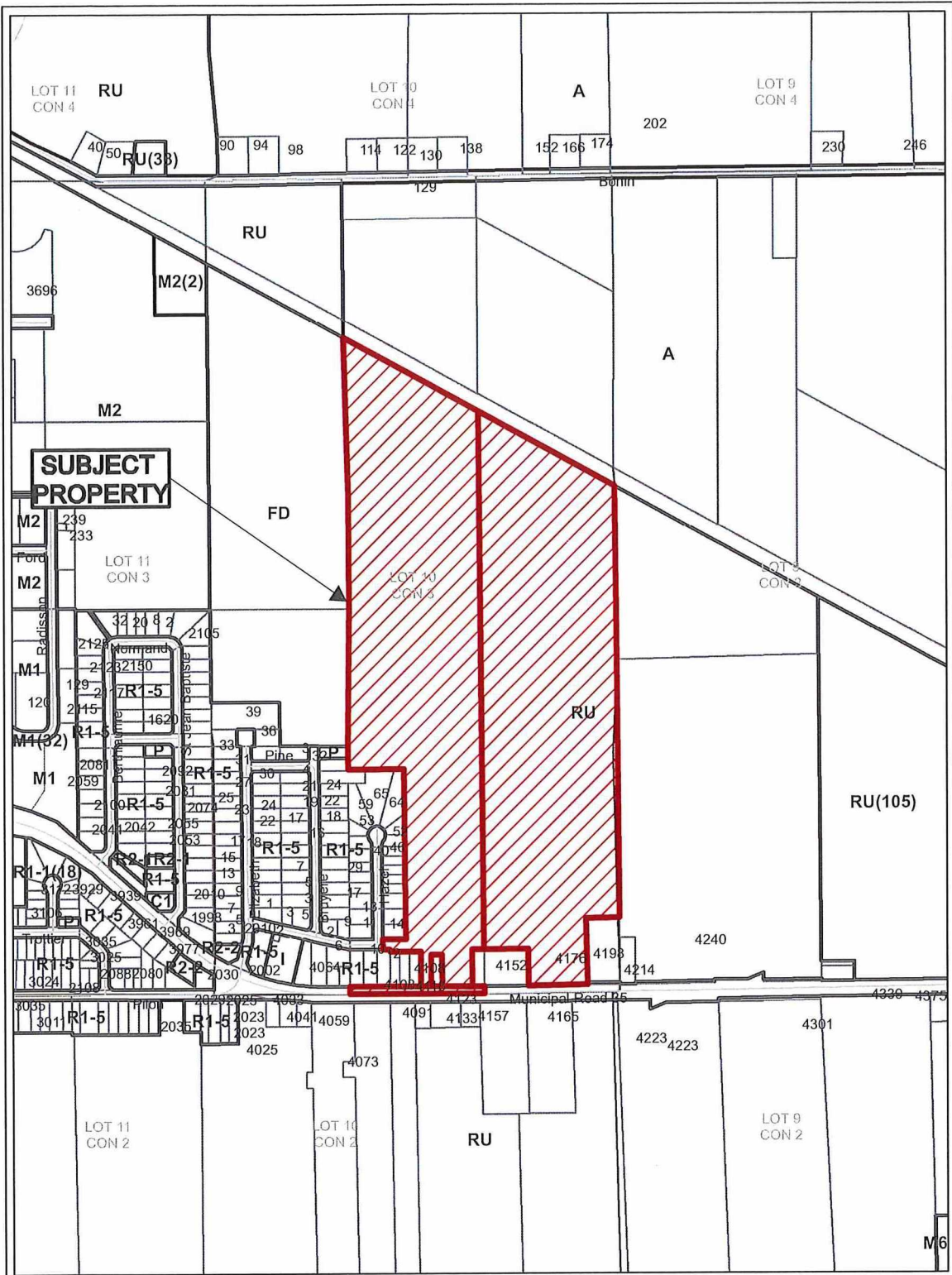
- Legal Property Description (GeoWarehouse Report);
- Site Plan; and
- Building Permit Drawings for Detached Garage.

We trust this submission package will enable the City of Greater Sudbury to schedule the subject application for the next available Committee of Adjustments meeting. Should there be any questions with respect to the above, please do not hesitate to contact the undersigned directly at [REDACTED]

Respectfully submitted,



Aaron Ariganello, BURP
Land Use Planner



Application for Minor Variance or Permission



Subject Property being PIN 73347-1440 & 73347-1438, SRO, Part Lot 10, Concession 3, Township of Rayside, as in LT5771 and LT19549, except LT17713, LT60738, LT125350, LT128292, LT146628, Parts 1-3, Plan 53R-5475, Parts 1-4, Plan 53R-8389, Parts 1-7, Plan 53R-18049, 4176 Municipal Road 35, Chelmsford, City of Greater Sudbury

Sketch 1, NTS
NDCA

PL-MV-2025-00118
Date: 2025 08 19

ENERGY EFFICIENCY - O.B.C. - SB12 - 2017 (HOUSE ONLY)

SB-12 Prescriptive (input design package); Package: A4 Table: 3.1.1.3.A

C. Project Design Conditions

Climate Zone (SB-17):	Heating Equipment Efficiency:	Space Heating Fuel Source:
Zone 1 (< 5000 degree days)	≥ 82% AFUE	Gas <input type="checkbox"/> Propane <input type="checkbox"/> Solid Fuel <input type="checkbox"/>
Zone 2 (> 5000 degree days)	≥ 84% AFUE	Oil <input type="checkbox"/> Electric <input type="checkbox"/> Earth Energy <input type="checkbox"/>

Ratio of Windows, Skylights & Glass (W, S & G) to Wall Area

Area of walls = $\frac{m^2 \text{ or } 2037.75 \text{ ft}^2}{m^2}$ W, S & G % = 12.4%

Area of W, S & G = $\frac{m^2 \text{ or } 442.66 \text{ ft}^2}{m^2}$

Other Building Characteristics

Log/Poist/Beam ICF Above Grade ICF Basement

Slab-on-ground Walkout Basement

Air Conditioning Combo Unit

Air Sourced Heat Pump (ASHP)

Ground Sourced Heat Pump (GSHP)

D. Building Specifications (provide values and ratings of the energy efficiency components proposed)

Energy Efficiency Substitutions

ICF (3.1.1.2.(5) & (6) / 3.1.1.3.(5) & (6))

Combined space heating and domestic water heating systems (3.1.1.2.(7) / 3.1.1.3.(7))

Airtightness substitution(s) Table 3.1.1.4.B. Required Permitted Substitution: _____

Airtightness test required (refer to Design Guide Attached) Table 3.1.1.4.C. Required Permitted Substitution: _____

Building Component	Minimum RSI / R values or Maximum U-value(s) ¹	Required	Permitted Substitution	Efficiency Ratings
Thermal Insulation				
Ceiling with Attic Space	R60	R63.22	Windows/Sliding Glass Doors	0.21 (1.2)
Ceiling without Attic Space	R43	R47.65	Skylights/Glazed Roofs	0.49 (2.6)
Exposed Floor	R31	R29.88	Mechanicals	
Walls Above Grade	R20/2.54	R23.90	Heating Equip.(AFUE)	90%
Basement Walls	R20/9	R21.12	HRV Efficiency (88% at 0°C)	65%
Slab (at >400mm below grade)	R10	R11.13	CHW Heater (COP)	8.07
Slab (edge only >200mm below grade)	R10	R11.13	DNHR (COP >1.1 min. 42% efficiency)	# Showers 2
Slab (at >400mm below grade, or heated)	R10	R11.13	Combined Heating System	

EEDS - WALL

ELEVATION	AREA
LL01 - L1A01	1150.91 SQ
LL02 - W0101	150.00 SQ
LL03 - W0201A	61.91 SQ
LL04 - W0201B	141.75 SQ

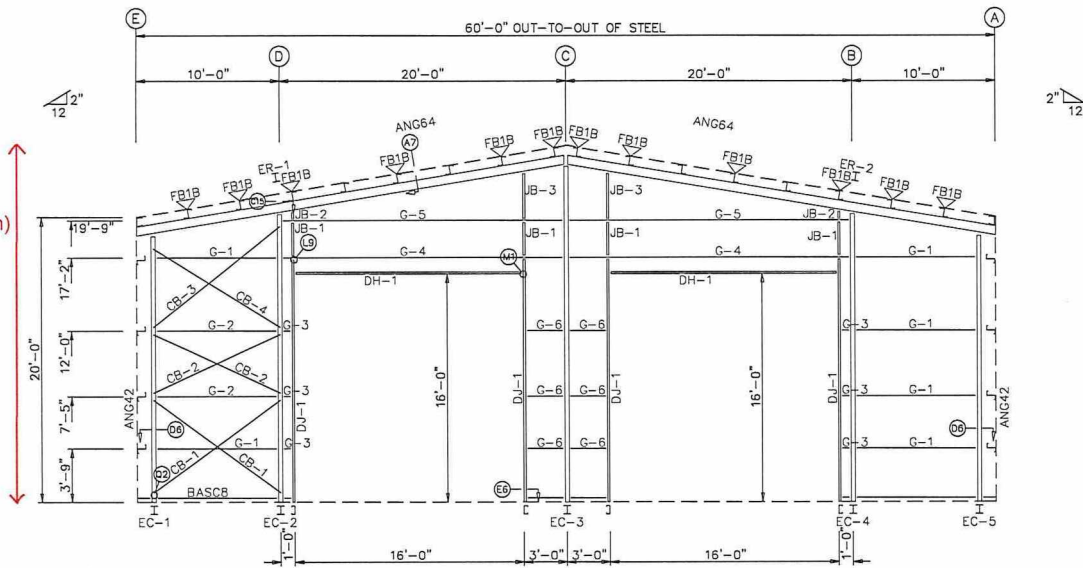
EEDS - WINDOWS

No.	WIDTH	HEIGHT	QTY	Area
W001	20"	42"	15	115.50 SQ
W002	40"	42"	1	168.00 SQ
W003	30"	42"	1	126.00 SQ
W004	24"	42"	1	100.80 SQ
W005	30"	42"	1	126.00 SQ
W006	30"	42"	1	126.00 SQ
W007	30"	42"	1	126.00 SQ
W008	30"	42"	1	126.00 SQ
W009	30"	42"	1	126.00 SQ
W010	30"	42"	1	126.00 SQ
W011	30"	42"	1	126.00 SQ
W012	30"	42"	1	126.00 SQ
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W014	30"	42"	1	126.00 SQ
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W017	30"	42"	1	126.00 SQ
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W100	30"	42"	1	126.00 SQ

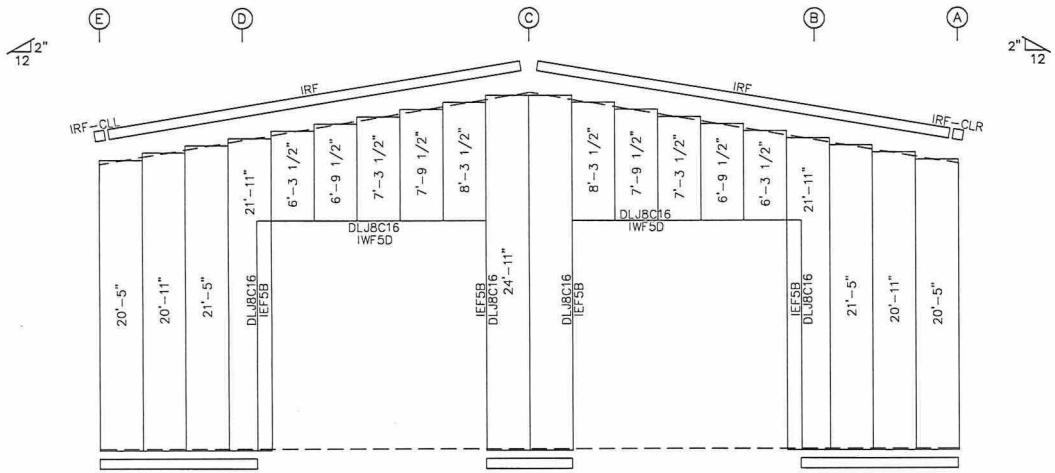
EEDS - DOORS

No.	TYPE	WIDTH	HEIGHT	AREA
D001	A	36"	84"	302.40 SQ
D002	A	36"	84"	302.40 SQ
D003	A	36"	84"	302.40 SQ
D004	A	36"	84"	302.40 SQ
D005	A	36"	84"	302.40 SQ
D006	A	36"	84"	302.40 SQ
D007	A	36"	84"	302.40 SQ
D008	A	36"	84"	302.40 SQ
D009	A	36"	84"	302.40 SQ
D010	A	36"	84"	302.40 SQ
D011	A	36"	84"	302.40 SQ
D012	A	36"	84"	302.40 SQ
D013	A	36"	84"	302.40 SQ
D014	A	36"	84"	302.40 SQ
D015	A	36"	84"	302.40 SQ
D016	A	36"	84"	302.40 SQ
D017	A	36"	84"	302.40 SQ
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D032	A	36"	84"	302.40 SQ
D033	A	36"	84"	302.40 SQ
D034	A	36"	84"	302.40 SQ
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D092	A	36"	84"	302.40 SQ
D093	A	36"	84"	302.40 SQ
D094	A	36"	84"	302.40 SQ
D095	A	36"	84"	302.40 SQ
D096	A	36"	84"	302.40 SQ
D097	A	36"	84"	302.40 SQ
D098	A	36"	84"	302.40 SQ
D099	A	36"	84"	302.40 SQ
D100	A	36"	84"	302.40 SQ

26' (7.92m)



ENDWALL FRAMING: FRAME LINE 1



ENDWALL SHEETING & TRIM: FRAME LINE 1
PANELS: 26 Ga. R - Charcoal Grey

GENERAL NOTES:

1. Use TEK5WW screws in place of SD150 panel screws at all 10 gage members.
2. See detail C7A for field coping of coldform endwall column flange braces.
3. All connections to door or window jambs where the clip is not designated in the clip table / drawing are made with JC# clips (# = Girt Depth).

BOLT TABLE				
FRAME LINE 1				
LOCATION	QUAN	TYPE	DIA	LENGTH
ER-1/ER-2	8	A325	1/2"	1 1/2"
Cor_Column/Raf	2	A325	3/4"	1 3/4"
EC-2/ER-1	2	A325	3/4"	1 3/4"
EC-3/ER-2	2	A325	1/2"	1 3/4"
EC-4/ER-2	2	A325	3/4"	1 3/4"


MEMBER TABLE			
FRAME LINE 1			
QUAN	MARK	PART	LENGTH
1	EC-1	W8X10	18'-5 5/16"
1	EC-2	W8X10	19'-11"
1	EC-3	W8X24	23'-0 5/8"
1	EC-4	W8X10	19'-11"
1	EC-5	W8X10	18'-5 5/16"
1	ER-1	W12X16	30'-4 15/16"
1	ER-2	W12X16	30'-4 15/16"
2	DJ-1	08X35C13	17'-1 3/4"
4	DH-1	08X35C16	16'-0"
2	G-1	08X25Z16	8'-5 1/2"
2	G-2	08X25Z13	8'-5 1/2"
2	G-3	08X25Z16	8'-5 1/2"
2	G-4	08X25Z13	15'-6 1/4"
2	G-5	08X25Z16	15'-6 1/4"
2	G-6	08X25Z16	2'-4 3/4"
2	G-7	08X25Z16	8'-11 5/8"
2	CB-1	CABLE250	7'-8 1/16"
2	CB-2	CABLE250	9'-2 11/16"
1	CB-3	CABLE250	8'-5 1/16"
1	CB-4	CABLE250	2'-6 1/2"
4	JB-1	08X35C16	2'-8 1/2"
2	JB-2	08X35C16	2'-8 1/2"
2	JB-3	08X35C16	2'-8 1/2"

FLANGE BRACE TABLE		
FRAME LINE 1		
VID	MARK	LENGTH
1	FB1B	1'-5 1/8"

DRAWING IS NOT TO SCALE

TRIM COLORS			
EAVE TRIM	= SP Black	CORNER TRIM	= SP Black
BASE TRIM	= Charcoal Grey	GUTTER	= SP Black
DOOR TRIM	= SP Black	DOWNSPOUTS	= SP Black
RAKE TRIM	= SP Black		
LINER TRIM	= Liner panel color		
SOFFIT TRIM	= Soffit panel color		

* ONLY APPLICABLE IF LINER TRIM OR SOFFIT PANEL IS INDICATED ON BUILDING ORDER.



Olympia Steel Buildings
2000 Highway 104, Suite 100, Yorkton, SK S9T 1K6
888-449-7756

JAMES ROBERT & JOSE ROBERT

60'-0" x 100'-0" x 20'-0"

DATE: 6/19/25 REVISION: 0

ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

CITY: CHELMSFORD ST/PR: ON

REVISION HISTORY

REV	DESCRIPTION	DATE

PL-MV-2025-00118

SKETCH 3

DRAWING STATUS

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FOR CONSTRUCTION: FINAL DRAWINGS.

JAMES ROBERT & JOSE ROBERT

REGISTERED PROFESSIONAL ENGINEER

A. IBRAHIM

90463316

07/15/25

PROVINCE OF ONTARIO

PAGE 12 OF 16

UNIVERSAL STEEL BUILDINGS

JAMES ROBERT & JOSEE ROBERT

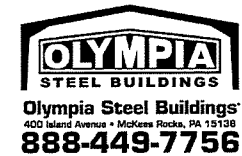
CHELMSFORD, ON P0M 1L0

FO# 30302

Building 1 of 1

SKETCH 4

PL-MV-2025-00118



INDEX OF DRAWINGS

Page	Drawing Title	REV NO.
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7	Roof Panel Layout	0
8	Rigid Frame #1	0
9	Rigid Frame #2	0
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Page	Drawing Title	REV NO.
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13	Right Endwall Framing	0
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32		0

GENERAL	
All materials included in the Metal Building System are in accordance with the manufacturer's standard materials and details unless otherwise specified on the order documents. (MBMA Common Industry Practices 2021, Section 2.1)	
DESIGN RESPONSIBILITY	
The manufacturer is responsible only for the structural design of the Metal Building System it sells to the purchaser / customer. Neither the manufacturer nor the manufacturer's engineer is the design professional or engineer of record for the construction project. The manufacturer is not responsible for the design of any component or materials not sold by it, or their interface and connection with Metal Building System unless such design responsibility is specifically required by the order documents. (MBMA Common Industry Practices 2021, Section 3.1) CSA A660 Manufacturer's Information: Corle Building Systems - CORLEO	
FOUNDATION DESIGN AND ANCHOR BOLTS	
The manufacturer is not responsible for the design, materials, and workmanship of the foundation. The anchor bolt plans prepared by the manufacturer are intended to show only the anchor bolt location, diameter (based on ASTM A36 bolts), and quantity required to connect the Metal Building System to the foundation. (MBMA Common Industry Practices 2021, Section 3.2.2). It is the responsibility of the end customer to ensure that adequate provisions are made for specifying bolt embedment, bearing angles, tie rods, and / or associated items embedded in the concrete foundation, as well as foundation design based on the loads imposed by the Metal Building System, or other imposed loads, and the bearing capacity of the soil and other conditions of the building site. (MBMA Common Industry Practices 2021, Section 3.2.2) U.S. -Anchor bolts shall be accurately set to a tolerance of +/- 1/8" in both elevation and location (AISC Code of Standard Practice for Steel Buildings and Bridges). Canada -Anchor bolts shall be accurately set in accordance with CISC Code of Standard Practice, December 2015, Clause 7.8.1	
ADJACENT EXISTING BUILDINGS	
The manufacturer does not investigate the influence of the Metal Building System on adjacent existing buildings or structures. The end customer assures that such buildings and structures are adequate to resist snow loads or other conditions as a result of the presence of the Metal Building System. (MBMA Common Industry Practices 2021, Section 3.2.5)	
SHOP-PRIMED STEEL	
All structural members of the Metal Building System not fabricated of corrosion resistant material or protected by corrosion resistant coating are painted with one coat of shop primer. All surfaces to receive shop primer are cleaned of loose rust, loose mill scale and other foreign matter by using, as a minimum the hand tool cleaning method SSPC-SP2 (Steel Manual, Structures Painting Council) prior to painting. The coat of shop primer is intended to protect the steel framing for only a short period of exposure to ordinary atmospheric conditions. Shop-primed steel should be placed on blocking to prevent contact with the ground, and so positioned as to minimize water holding pockets, dust, mud or other contamination of the primer film. Repairs of damage to primed surfaces and or removal of foreign material due to improper field storage or site conditions are not the responsibility of the manufacturer. (CISC Code of Standard Practice, December 2015, Clauses 6.8 & 6.9; (MBMA Common Industry Practices 2021, Section 4.2.4).	
ERECTION-GENERAL	
The erector, by entering into contract to erect the building, holds itself out as skilled in the erection of Metal Building Systems and is responsible for complying with all applicable local, federal, and state construction and safety regulations including OSHA regulations as well as any applicable requirements of local, national, or international union rules or practices. (CISC Code of Standard Practice, December 2015, Clause 7.3; (MBMA Common Industry Practices 2021, Section 6.9). The erector shall erect the Metal Building System in accordance with the erection drawings, the Erection and Detail Manual (2019), and / or the Seam-Lok Technical - Erection manual (2019) as furnished by the manufacturer. The aforementioned erection information is intended to illustrate the layout of the framing members, provide the associated connection details, and suggests sequence of erection. It is not intended to specify any particular method of erection to be followed by the erector. The erector remains solely responsible for the safety and appropriateness of all techniques and methods utilized by its crews in the erection of the Metal Building System. The erector is responsible for supplying any safety devices such as scaffolds, runways, nets, etc, which may be required to safely erect the Metal Building System. (MBMA Common Industry Practices 2021, Section 6.9) The manufacturer expressly disclaims any responsibility for injury to persons in the course of erection or for damages to the product itself. Field erection of a Pre-Engineered Metal Building, as in all construction projects, involves hazards to persons within the area of the construction and risk of damage to the property itself. Only experienced persons who are skilled and qualified in the erection of Metal Building Systems should be permitted to field-erect a building due to the hazards of this construction activity. The manufacturer is not responsible for the erection of the Metal Building System, the supply of any tools or equipment, or any other field work. The manufacturer provides no field supervision for the erection of the structure nor does the manufacturer perform any intermediate or final inspections of the Metal Building System during or after erection. The erector shall furnish temporary guys and bracing where needed for squaring, plumbing, and securing the structural framing against loads, such as wind loads acting on the exposed framing as well as loads due to erection equipment and erection operation, but not including loads resulting from the performance of work by others. Bracing furnished by the manufacturer for the Metal Building System cannot be assumed to be adequate during erection. Temporary supports such as temporary guys, braces, false work, cribbing, or other elements required for the erection operation will be determined, erected, and installed by the erector. (AISC Code of Standard Practice for Steel Buildings and Bridges, June 15, 2016, Section 7.10.3; CISC Code of Standard Practices, December 2015, Clause 1.5; MBMA Common Industry Practices 2021, Section 6.2.1.5).	
ERECTION TOLERANCES	
U.S. : Erection tolerances are those set forth in AISC code of standard practice except individual members are considered, plumb, level and aligned if the deviation does not exceed 1:500. (AISC Code of Standard Practice for Steel Buildings and Bridges June 15, 2016 Section 7.13.1; MBMA Common Industry Practices 2021, Section 6.8) Canada; Erection tolerances are those set forth in CISC Code of Standard Practice except individual members are considered plumb, level and aligned if the deviation does not exceed 1:500. (CISC Handbook of Steel Construction, Eleventh Edition, Third Revised Printing, Part 1, Clause 29.3; MBMA Common Industry Practices 2021, Section 6.8)	
BOLT TIGHTENING	
The proper tightening and inspection of all fasteners is the responsibility of the erector (Reference RCSC for structural joints using high strength bolts; June 11, 2020). All high strength (ASTM F3125, A325, A490) bolts and nuts must be tightened by the "turn-of-the-nut" method unless otherwise specified by the end customer in the contract documents. Inspection of high strength bolt and nut installation by other than the erector must also be specified in the contract documents and the erector is responsible for ensuring that the installation procedures are compatible prior to the start of erection (CISC Handbook of Steel Construction, Eleventh Edition, Third Revised Printing, Part 1, Clause 23.7.2), (MBMA Common Industry Practices 2021, Section 6.9).	

MATERIALS	ASTM DESIGNATION	MINIMUM YIELD	MATERIALS	ASTM DESIGNATION	MINIMUM YIELD
Hot-Rolled Mill Sections	A 36, A 572, A 592	Fy = 36 ksi and/or 50 ksi	Roof and Wall Sheeting	A 792, Gr. 50 Class 1 A 792, Gr. 80	Fy = 50 ksi Fy = 80 ksi
Structural Steel Plates	A 572, A 1011	Fy = 55 ksi	Mild Steel Bolts	A 307	Fy = 36 ksi
Structural Steel Bars	A 572 or A 529	Fy = 55 ksi	High Strength Bolts	F3125: A 325-N A 490-N	Fy = 92 or 81 ksi N/A
Cold Formed Light Gauge Shapes	A 653 Gr. 55	Fy = 55 ksi	Anchor Rods (If supplied)	A 36	Fy = 36 ksi
Cable Bracing	A 475, EHS	N/A	Pipe and Hollow Structural Sections	A 500 Gr. B	Fy = 42 ksi, 46 ksi
Rod Bracing	A 36	Fy = 36 ksi			

CORRECTION OF ERRORS AND REPAIRS
The correction of minor misfits by the use of drift pins to draw the components into line, shimming, moderate amounts of reaming, chipping, and cutting, and the replacement of minor shortages of material are a normal part of erection and are not subject to claim. (AISC Code of Standard Practice for Steel Buildings and Bridges, June 15, 2016, Section 7.14; CISC Code of Standard Practice, December 2015, Clause 7.15; MBMA Common Industry Practices 2021, Section 6.10).

DRAWING DISCREPANCIES
In case of discrepancies between the manufacturers steel plans and plans for other trades, the manufacturers steel plans govern. (AISC Code of Standard Practice for Steel Buildings and Bridges, June 15, 2016, Section 3.3; CISC Code of Standard Practice, December 2015, Clause 3.4; MBMA Common Industry Practices 2021, Section 3.1).

DELIVERIES
Delivery of any material by the manufacturers carrier, a common carrier, or to purchasers/ customers own leased, chartered, or authorized conveyance shall constitute delivery to builder, and thereafter, such material shall be at builders risk. If builder chooses to use its own, or private carrier, it shall be solely responsible for compliance with all applicable government regulations. All charges shall be borne by the builder. The manufacturers responsibility for damage or loss ceases upon delivery of shipment to carrier. The manufacturer will endeavor to deliver on the required date. The manufacturers truck is not considered as being late if deliveries are between 6am - 12pm (morning) and 12pm - 5pm (afternoon). However, the manufacturer cannot be held responsible for circumstances beyond our control. For deliveries via the manufacturers truck, the manufacturer will only honor claims that were approved by the customer service department at the time of delivery. For deliveries via contract carriers, it is the responsibility of the customer to file claims with the carrier. The manufacturer cannot assume any liability for the claim.


SHORTAGES
The purchaser /customer should make an inspection upon arrival of all building components. The purchaser/customer must note on the freight bill any missing item(s) and notify the manufacturers customer service department immediately; otherwise, the manufacturer cannot be held responsible for any shortages. If any item is damaged, note on the bill of lading and file a claim with the freight agent. Concealed shortages must be reported to the manufacturers customer service department within the following time frames (date from receipt of first delivery), based on the project shipment size, i.e., number of truck loads used in delivery.
1 to 3 loads_2 weeks 4 loads and over_3 weeks The manufacturers responsibility for shortages expires at the end of these time periods.

FABRICATION ERRORS
The purchaser/customer is responsible for contacting the customer service department to advise the manufacturer of fabrication problems and corresponding cost estimates. The manufacturer will be responsible for providing the builder with verbal approval to proceed with appropriate field corrections. This will be done in a timely manner. IF THE BUILDER PROCEEDS WITH CORRECTIVE WORK WITHOUT THE MANUFACTURERS APPROVAL, HE DOES SO AT HIS OWN RISK. The manufacturer shall not be responsible for any claims where the purchaser/customer has not documented the problem, its correction, and reasonable costs for repair, and submitted this documentation for payment within 30 days of the occurrence.

INVOICE PAYMENT
By acceptance of the materials of services set forth in the invoice, the purchaser/customer agrees to pay the invoice amount within the time period specified on the invoice. AT NO TIME IS IT ACCEPTABLE TO DEDUCT A BACK CHARGE OR SHORTAGE FROM AN INVOICE.

SAFETY PROCEDURES
The manufacturer is committed to manufacturing a quality product that can be erected safely. Although good job site practices and a commitment to safety by the erector are beyond the control of the manufacturer, the manufacturer highly recommends the erector provide good, safe working conditions on the job site. The erector should follow all local, state, and federal health and safety regulations at all times. Accident prevention practices should be implemented and each employee should know emergency procedures. The manufacturer also recommends daily meetings to discuss erection safety procedures. For additional information concerning federal health and safety regulations, contact the occupational safety and health administration (osha).
U.S. Department of Labor
Occupational Safety and Health Administration
200 Constitution Avenue, N.W.
Washington, DC 20210
www.osha.gov

The manufacturer shall not be responsible for personal injury or property damage as a result of failure to follow all applicable safety regulations and material handling and installation recommendations.



OLYMPIA STEEL BUILDINGS
Olympia Steel Buildings
2000 West 10th Street
Vancouver, BC V6P 6E6
Canada
888-449-7756

JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
DATE: 6/19/25 REVISION: 0
ENG: C.J.A. DWN: B.J.C. APPD: C.J.A.

F.O. 30302

REV	DESCRIPTION

PL-MV-2025-00118
SKETCH 5


JAMES ROBERT & JOSE ROBERT
CITY: CHELMSFORD
REV: 0
DATE: 6/19/25

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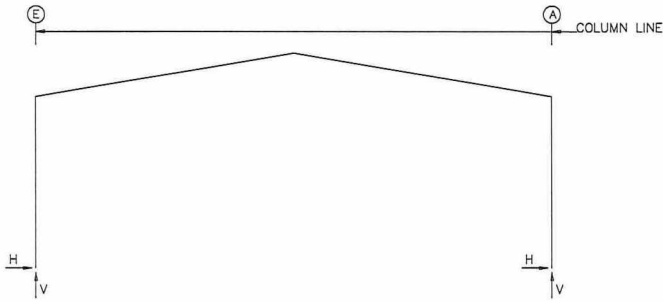
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REGISTERED PROFESSIONAL ENGINEER
A. IBRAHIM
90463316
07/15/25
PROVINCE OF ONTARIO

PAGE 1 OF 16

FRAME LINES: 2 3 4 5



RIGID FRAME: BASIC COLUMN REACTIONS (k)

Frame Line	Column Line	Horz	Vert	Dead	Callateral	Live	Snow	Wind_Left1	Wind_Right1
2*	E	1.1	2.8	0.9	1.8	6.4	12.6	15.2	30.1
2*	A	-1.1	2.8	-0.9	1.8	-6.4	12.6	-15.2	30.1
2*	E	-6.0	-4.1	0.3	-0.1	-2.3	-12.3	-3.1	-10.2
2*	A	-0.3	-0.1	6.0	-4.1	3.1	-10.2	2.3	-12.3
2*	E	11.4	26.5	11.4	18.7				
2*	A	-11.4	18.7	-11.4	26.5				
3*	E	1.1	2.8	0.9	1.8	6.4	12.6	15.2	30.1
3*	A	-1.1	2.8	-0.9	1.8	-6.4	12.6	-15.2	30.1
3*	E	-4.6	-2.2	0.1	0.8	-1.2	-13.0	-1.7	-11.7
3*	A	-0.1	0.8	4.6	-2.2	1.7	-11.7	1.2	-13.0
3*	E	0.0	-9.4	11.4	26.5				
3*	A	0.0	-9.4	-11.4	18.7				
2*	Frame lines:	2	5						
3*	Frame lines:	3	4						

RIGID FRAME: ANCHOR BOLTS & BASE PLATES

Frm Line	Col Line	Anc. Bolt Qty	Bolt Dia	Base Plate Width	Base Plate Length	Base Plate Thick	ARE/BRE (in)
2*	E	4	0.750	8.000	17.63	0.500	0.0
2*	A	4	0.750	8.000	17.63	0.500	0.0
2*	Frame lines:	2	5				

RIGID FRAME: ANCHOR BOLTS & BASE PLATES

Frm Line	Col Line	Anc. Bolt Qty	Bolt Dia	Base Plate Width	Base Plate Length	Base Plate Thick	ARE/BRE (in)
3*	E	4	0.750	8.000	17.63	0.500	0.0
3*	A	4	0.750	8.000	17.63	0.500	0.0
3*	Frame lines:	3	4				



Olympia Steel Buildings
888-449-7756

JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"

DATE: 6/19/25 REVISION: 0

ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

JAMES ROBERT & JOSE ROBERT
CITY: CHELMSFORD ST./PV: ON

REV	DESCRIPTION	DATE

PL-MV-2025-00118
SKETCH 7

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FOR CONSTRUCTION: FINAL DRAWINGS.

REGISTERED PROFESSIONAL ENGINEER
A. IBRAHIM
90463316
07/15/25
PROVINCE OF ONTARIO

ENDWALL COLUMN: BASIC COLUMN REACTIONS (k)

Frm Line	Col Line	Dead Vert	Collat Vert	Live Vert	Snow Vert	Wind_Left1 Horz	Wind_Right1 Horz	Wind_Left2 Horz	Wind_Right2 Horz	Wind Press Horz
1	D	0.3	0.1	0.6	1.3	-1.6	-3.8	0.0	2.9	-1.6
1	E	0.9	0.5	3.6	8.5	0.0	-1.1	1.6	-5.8	0.0
1	C	1.4	0.7	4.6	11.0	0.0	-3.6	0.0	-4.1	0.0
1	B	0.9	0.5	3.6	8.5	0.0	-2.5	0.0	-4.2	0.0
1	A	0.3	0.1	0.6	1.3	0.0	-0.7	0.0	-0.5	0.0

Frm Line	Col Line	Wind Suct Horz	Wind_Long1 Horz	Wind_Long2 Horz	Seis_Left Horz	Seis_Right Horz	Seis Long Horz	E1UNB_SL_L Horz
1	E	0.6	0.0	0.3	-0.5	-1.6	-2.0	-4.3
1	D	1.5	0.5	-5.2	0.0	-1.4	0.0	4.1
1	C	2.4	0.0	-3.5	0.0	-3.4	0.0	0.3
1	B	1.5	0.0	-2.3	0.0	-4.2	0.0	-0.1
1	A	0.6	0.0	-0.7	0.0	-0.8	0.0	0.0

Frm Line	Col Line	Dead Vert	Collat Vert	Live Vert	Snow Vert	Wind_Left1 Horz	Wind_Right1 Horz	Wind_Left2 Horz	Wind_Right2 Horz	Wind Press Horz
6	A	0.3	0.1	0.6	1.3	-1.6	-3.8	0.0	2.9	-1.6
6	E	0.9	0.5	3.6	8.5	0.0	-1.1	1.6	-5.8	0.0
6	C	1.4	0.7	4.6	11.0	0.0	-3.6	0.0	-4.1	0.0
6	D	0.9	0.5	3.6	8.5	0.0	-2.5	0.0	-4.2	0.0
6	B	0.3	0.1	0.6	1.3	0.0	-0.7	0.0	-0.5	0.0

Frm Line	Col Line	Wind Suct Horz	Wind_Long1 Horz	Wind_Long2 Horz	Seis_Left Horz	Seis_Right Horz	Seis Long Horz	E2UNB_SL_L Horz
6	A	0.6	0.0	0.3	-0.5	-1.6	-2.0	-4.3
6	E	1.5	0.5	-5.2	0.0	-1.4	0.0	4.1
6	C	2.4	0.0	-3.5	0.0	-3.4	0.0	0.3
6	D	1.5	0.0	-2.3	0.0	-4.2	0.0	-0.1
6	B	0.6	0.0	-0.7	0.0	-0.8	0.0	0.0

Frm Line	Col Line	E2UNB_SL_R Vert
6	A	0.0
6	E	0.8
6	C	3.8
6	D	0.0
6	B	0.0
6	A	1.1

ANCHOR BOLT SUMMARY

Qty	Locate	Dia (in)	Type
@ 40	Endwall	3/4"	
@ 32	Frame	3/4"	

BUILDING BRACING REACTIONS

Wall Loc	Col Line	Reactions in plane of wall ± Reactions(k)	Panel Shear (lb/ft)		
Line	Line	Wind Horz	Seismic Vert	Wind Horz	Seismic Vert
L_EW	1	E,D	Bracing, see EW reactions		
F_SW	A	3,4	4,3	10,7	*
R_EW	E	A,B	Bracing, see EW reactions		
B_SW	E	4,3	4,3	10,7	*

*See RF reactions table for vertical and horizontal reactions in plane of the rigid frame.
Reactions for seismic represent shear force, V
Reaction values shown are unfactored

ENDWALL COLUMN: ANCHOR BOLTS & BASE PLATES

Frm Line	Col Line	Anc. Bolt Qty Dia	Base Plate Width	Base Plate Length	Base Plate Thickness	ARE/BRE (in)
1	E	4 0.750	6.000	7.875	0.375	0.0
1	D	4 0.750	6.000	7.875	0.375	0.0
1	C	4 0.750	6.000	7.875	0.375	0.0
1	B	4 0.750	6.000	7.875	0.375	0.0
1	A	4 0.750	6.000	7.875	0.375	0.0

Reaction Type	Description
Dead	Dead Load
Collateral	Collateral Load
Live	Roof Live Load
Snow	Uniform Roof Snow Load
Wind_Left1	Transverse Wind From Left With Internal Pressure
Wind_Right1	Transverse Wind From Right With Internal Pressure
Wind_Left2	Transverse Wind From Left With Internal Suction
Wind_Right2	Transverse Wind From Right With Internal Suction
Wind_Long1	Longitudinal Wind From Left Endwall
Wind_Long2	Longitudinal Wind From Right Endwall
Wind_Press	Wind Pressure Applied Towards Endwall
Wind_Suct	Wind Suction Applied Away From Endwall
Seismic_Left	Seismic Load Applied From Left
Seismic_Right	Seismic Load Applied From Right
Seismic_Long	Seismic Load Applied Longitudinally
F# Unb_SL_L	Unbalanced Snow Load On Left
F# Unb_SL_R	Unbalanced Snow Load On Right
F# PAT_SL#	Pattern Snow Load
Drift	Snow Drift Load
Sliding	Sliding Snow Load
HorzOP	Horizontal Out Of Plane Load

+DESIGN INFORMATION

- All loading conditions are examined and only the maximum / minimum H or V and the corresponding H or V are reported.
- Positive reactions are shown in the sketch. Foundation loads are in opposite directions.
- Bracing reactions are in the plane of the brace with the H pointing away from the braced bay. The vertical reaction is downward.
- Building reactions are based on the following building data:

DESIGN CRITERIA	SEISMIC CRITERIA	DEFLECTION LIMITS
Width (ft) = 60	Seismic Importance = 1.00	ENDWALL COLUMN L / 180
Length (ft) = 100	Risk Category = II - Normal	ENDWALL RAFTER (Live) L / 180
Eave Height (ft) = 20	Mapped Spectral Response Accelerations	ENDWALL RAFTER (Wind) L / 180
Roof Slope (rise/12) = 2.0:12	Sa (0.2,X) = 0.2550	WALL GIRTS L / 180
Building Code = NBC 20	Sa (0.5,X) = 0.2570	PURLIN (LIVE) L / 90
Local Code (State/Prov) = ONBC 24	Sa (1.0,X) = 0.1540	PURLIN (WIND) L / 120
Dead Load (psf) = 2.25	Sa (2.0,X) = 0.0736	WALL PANEL L / 90
Collateral Load (psf) = 3.01	Sa (5.0,X) = 0.0193	ROOF PANEL (Live) L / 180
Roof Live Load (psf) = 21.00	Sa (10.0,X) = 0.0061	ROOF PANEL (Wind) L / 120
Frame Live Load (psf) = 21.00	Site Class = D	Main Frame (Horiz) H / 60
Snow: Ground Snow Load (psf) = 52.25	Expanded Formula = S(Ta)*Mv*le*W/(Rd*Ro)	Main Frame (Vert) H / 60
Snow Importance = 1.0000	Longitudinal Base Shear (k) = 21.33	WIND BRACING H / 60
Associated Rain Load (psf) = 6.36	Transverse Base Shear (k) = 17.80	Main Frame (Crane) H / 100
Wind Exposure Factor = 1.00	Seismic Response Coefficients--	Main Frame (Seismic) H / 100
Slippery Roof = N	Frame = 0.132	SEISMIC BRACING H / 40
Roof Snow Load (psf) = 50.16	FSW = 0.132	PARTITION COLUMN L / 120
Wind: Wind (1/50) (psf) = 9.61	BSW = 0.132	PARTITION GIRT L / 120
Risk Category = II - Normal	Response Modification Factors--	PARTITION PANEL L / 120
Importance - Wind = 1.00	Frame = 1.5	
Wind Exposure = N	FSW = 1.5	
Enclosure Classification = 2	BSW = 1.5	
Internal Pressure Coefficients--		
Pressure = 0.30		
Suction = -0.45		
Components & Cladding--		
Design Pressure: Pressure (psf) = 24.06		
Suction (psf) = -24.06		
Equivalent Static Force Procedure.		

JAMES ROBERT & JOSE ROBERT
STEEL BUILDINGS
Olympia Steel Buildings
888-449-7756

JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
DATE: 6/19/25 REVISION: 0
ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

REV	DESCRIPTION	DATE
	REVISION HISTORY	

JAMES ROBERT & JOSE ROBERT
CITY: CHELMSFORD ST/PV: ON

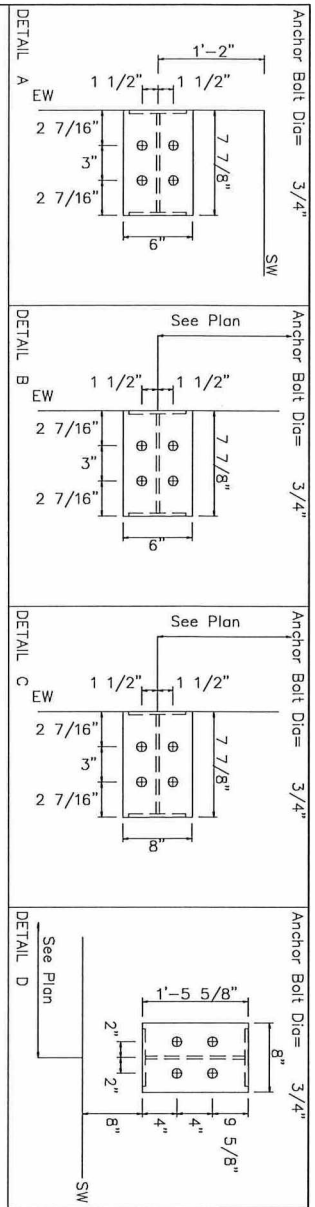
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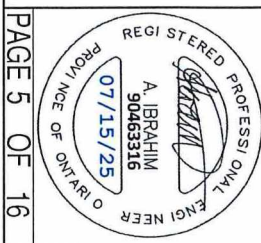
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PL-MV-2025-00118
SKETCH 8

REGISTERED PROFESSIONAL ENGINEER
A. IBRAHIM
90463316
07/15/25
PROVINCE OF ONTARIO



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<h3>JAMES ROBERT & JOSEE ROBERT</h3> <p>DRAWING STATUS</p> <p><input type="checkbox"/> FOR APPROVAL: THESE DRAWINGS, BEING FOR APPROVAL, ARE BY DEFINITION NOT FINAL, AND ARE FOR CONCEPTUAL REPRESENTATION ONLY. THEIR PURPOSE IS TO CONFIRM PROPER INTERPRETATION OF THE PROJECT DOCUMENTS. ONLY DRAWINGS ISSUED "FOR CONSTRUCTION" CAN BE CONSIDERED AS COMPLETE.</p> <p><input checked="" type="checkbox"/> FOR PERMIT: THESE DRAWINGS, BEING FOR PERMIT, ARE BY DEFINITION NOT FINAL, ONLY DRAWINGS ISSUED "FOR CONSTRUCTION" CAN BE CONSIDERED AS COMPLETE.</p> <p><input type="checkbox"/> FOR CONSTRUCTION: FINAL DRAWINGS.</p>		<p>CITY: CHELMSFORD ST./PV: ON</p> <p>REVISION HISTORY</p> <table border="1"> <thead> <tr> <th>REV.</th> <th>DESCRIPTION</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td></td> <td>PL-MV-2025-00118 SKETCH 9</td> <td></td> </tr> </tbody> </table>	REV.	DESCRIPTION	DATE		PL-MV-2025-00118 SKETCH 9	
REV.	DESCRIPTION	DATE						
	PL-MV-2025-00118 SKETCH 9							

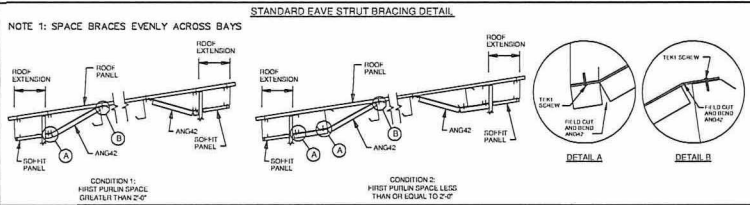
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OLYMPIA STEEL BUILDINGS
 422 Industrial Avenue • Markham, Ontario, M3B 1Y7
 888-449-7756

JAMES ROBERT & JOSEE ROBERT

60'-0" x 100'-0" x 20'-0"

DATE: 6/19/25	REVISION: 0
ENG: CJA	DWN: BJC
APPD: CJA	



MEMBER TABLE			
ROOF PLAN			
QUAN	MARK	PART	LENGTH
16	P-1	08X25Z13	23'-1 1/2"
16	P-2	08X25Z14	25'-3 1/2"
16	P-3	08X25Z14	24'-3 1/2"
16	P-4	08X25Z14	25'-3 1/2"
16	P-5	08X25Z13	23'-1 1/2"
2	F-1	08X35E13	19'-11 1/2"
2	F-2	08X35E13	19'-11 1/2"
2	F-3	08X35E13	19'-11 1/2"
4	CB-6	CABLE375	20'-1 9/16"
4	CB-7	CABLE250	25'-4 5/16"

SPECIAL BOLTS				
ROOF PLAN				
Q ID	QUAN	TYPE	DIA	LENGTH WASH
1	4	A325	1/2"	1 1/4" 0



JAMES ROBERT & JOSEE ROBERT
 60'-0" x 100'-0" x 20'-0"
 DATE: 6/19/25 REVISION: 0
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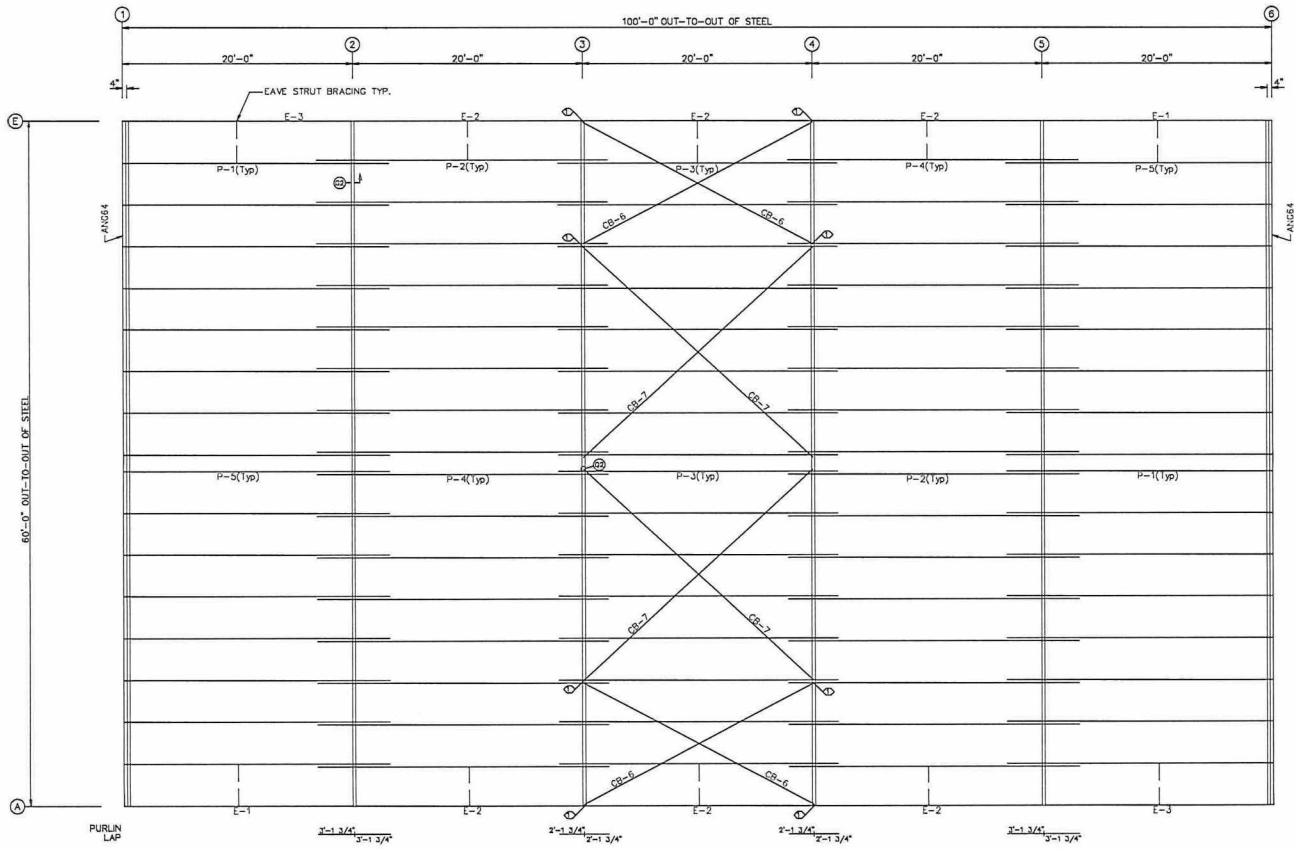
JAMES ROBERT & JOSEE ROBERT
 CITY: CHELMSFORD ST/PV: ON

REV#	DESCRIPTION	DATE

PL-MV-2025-00118
 SKETCH 10

DRAWING STATUS

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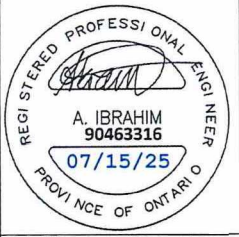


ROOF FRAMING PLAN

GENERAL NOTES:

1. Screw Down Roof: Use TEK5WW screws in place of SD150 panel screws at all 10 gage purlins, eave struts, or roof joists.
2. Standing Seam Roof: Use FST#6 in place of FST#1 clip to purlin screws at all 10 gage purlins, eave struts, or at roof joists.

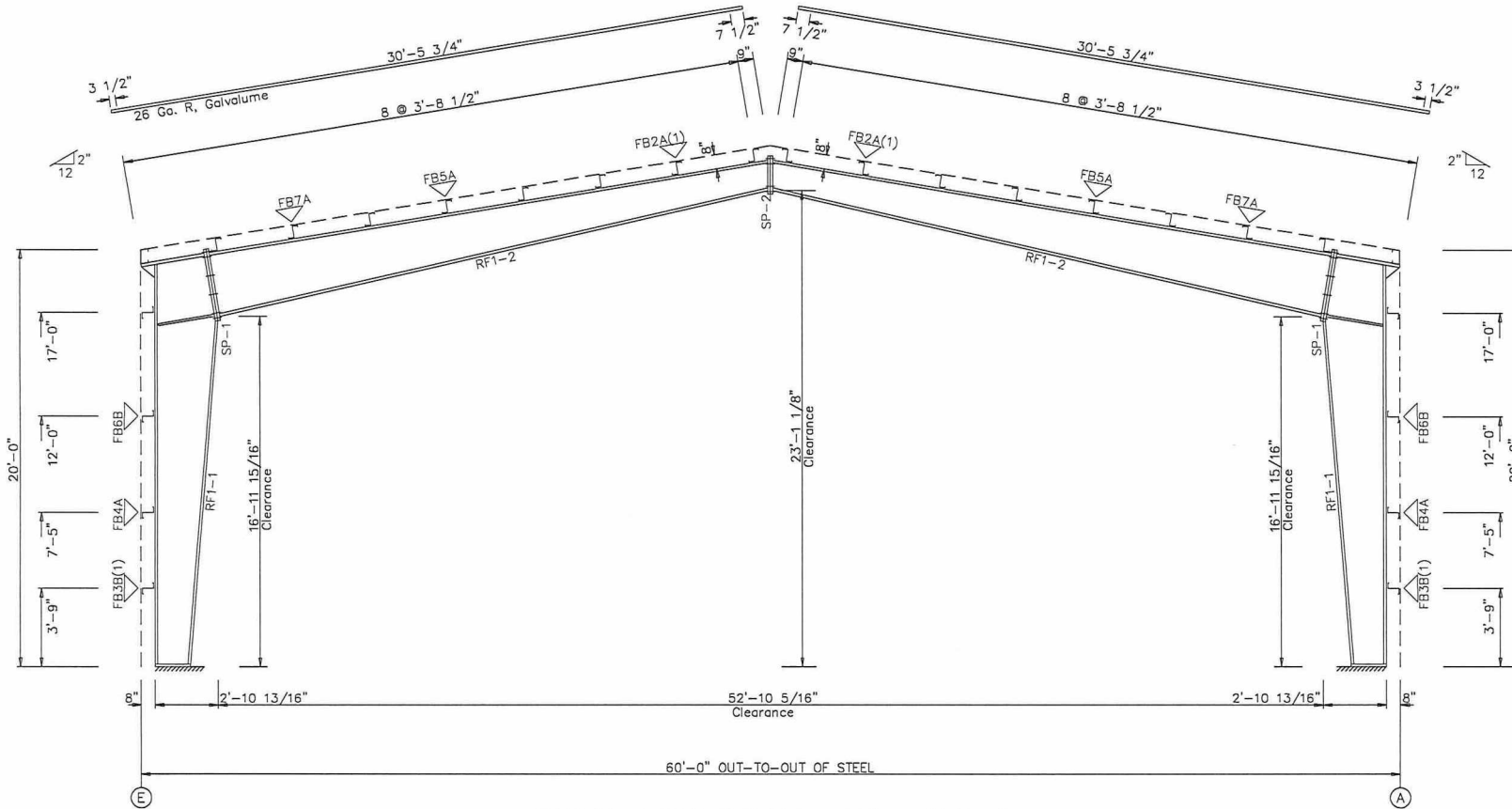
DRAWING IS NOT TO SCALE



SPLICE BOLT TABLE						
Mark	Qty		Type	Dia	Length	
	Top	Bot			Int	
SP-1	4	4	A325	1.000	2.75	
SP-2	4	4	A325	0.750	2.00	

MEMBER TABLE										
Mark	Web		Plate	Outside Flange			Inside Flange			
	Start	End		Thick	Length	W	Thk	Length	W	Thk
RF1-1	17.0	34.0	0.188	237.1	6	1/4"	232.5	6	3/8"	120.4
RF1-2	34.0	21.3	0.250	206.6	6	1/4"	36.4	6	5/8"	79.6
	21.3	14.0	0.149	120.0	6	1/4"	206.6	6	3/8"	86.7
					6	1/4"	120.0	6	1/4"	238.1

▽ FLANGE BRACES: Both Sides(U.N.)
 FBxxB(1)
 B - L20X1/4
 A - L15X1/8



BUILDING CROSS SECTION: FRAME LINE 2 5

GENERAL NOTES:

1. See Detail Sheets for Connection Information.
2. See Shipping List for Flange Brace Lengths.

DRAWING IS NOT TO SCALE



JAMES ROBERT & JOSE ROBERT
 60'-0" x 100'-0" x 20'-0"
 DATE: 6/19/25 REVISION: 0
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JAMES ROBERT & JOSE ROBERT
 CITY: CHELMSFORD ST./PV: ON

REV.	DESCRIPTION	DATE
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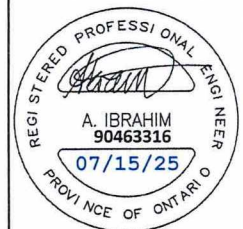
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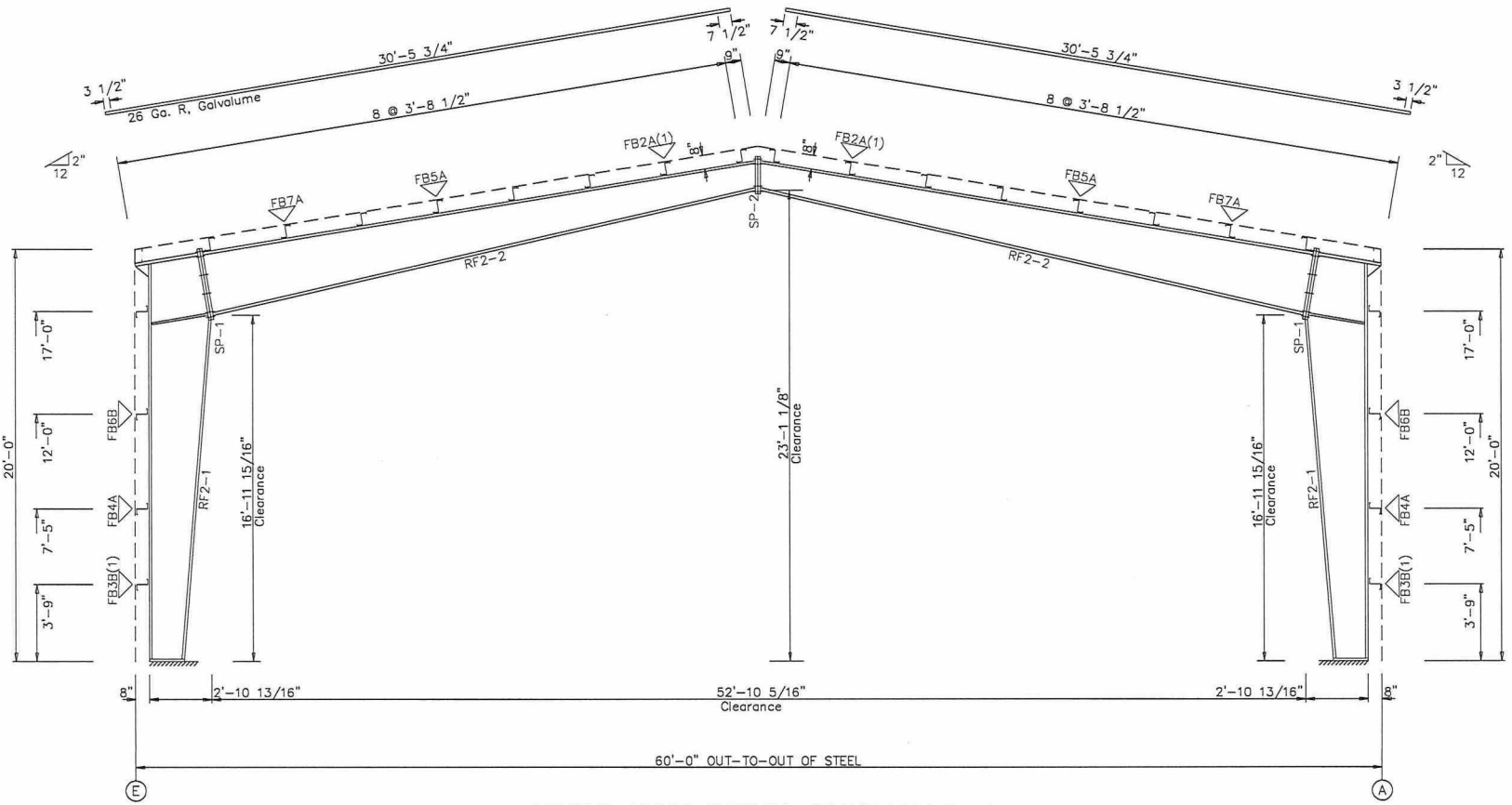
PL-MV-2025-00118
 SKETCH 12



SPlice Bolt Table						
Mark	Qty		Int	Type	Dia	Length
	Top	Bot				
SP-1	4	4	4	A325	1.000	2.75
SP-2	4	4	0	A325	0.750	2.00

MEMBER TABLE								
Mark	Web Depth		Web Plate		Outside Flange		Inside Flange	
	Start	End	Thick	Length	W x Thk	x Length	W x Thk	x Length
RF2-1	17.0	34.0	0.188	237.1	6 x 1/4"	x 232.5	6 x 3/8"	x 120.4
RF2-2	34.0	21.3	0.250	206.6	8 x 1/4"	x 36.4	8 x 5/8"	x 79.6
	21.3	14.0	0.149	120.0	6 x 1/4"	x 206.6	6 x 3/8"	x 86.7
					6 x 5/16"	x 120.0	6 x 1/4"	x 238.1

▽ FLANGE BRACES: Both Sides(U.N.)
 FBxxB(1)
 B - L20X1/4
 A - L15X1/8



BUILDING CROSS SECTION: FRAME LINE 3 4

GENERAL NOTES:
 1. See Detail Sheets for Connection Information.
 2. See Shipping List for Flange Brace Lengths.

DRAWING IS NOT TO SCALE



JAMES ROBERT & JOSEF ROBERT
 60'-0" x 100'-0" x 20'-0"
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F.O. 30302

JAMES ROBERT & JOSEF ROBERT
 CITY: CHELMSFORD ST./PV: ON

REV	DESCRIPTION	DATE

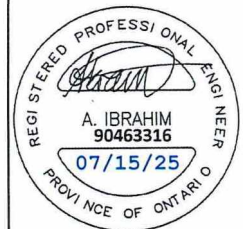
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 SKETCH 13

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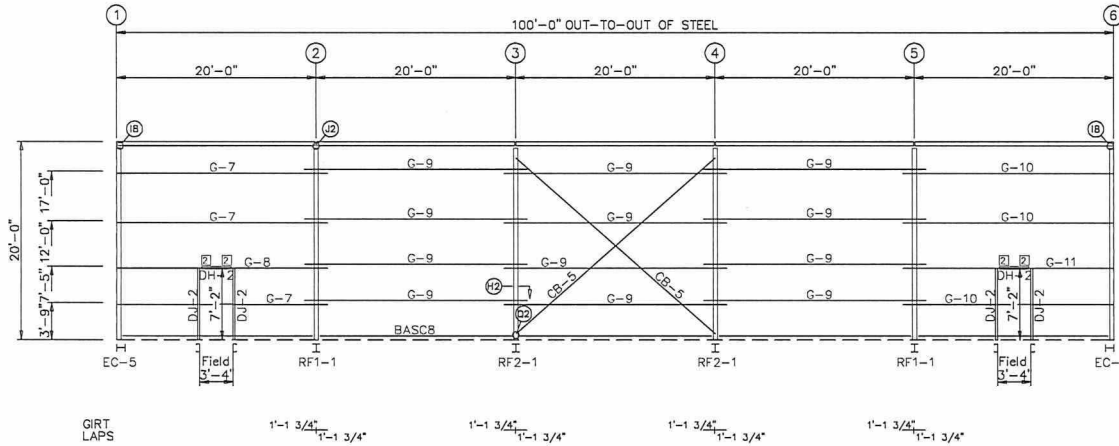
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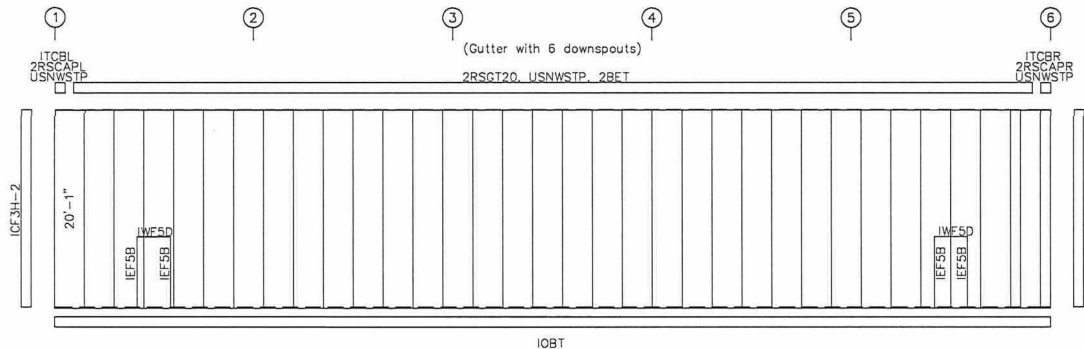


MEMBER TABLE FRAME LINE A			
QUAN	MARK	PART	LENGTH
4	DJ-2	08X35C16	7'-4 3/4"
2	DH-2	08X30C16	3'-4"
3	G-7	08X25Z16	21'-1 1/2"
1	G-8	08X25Z14	21'-1 1/2"
12	G-9	08X25Z16	22'-3 1/2"
3	G-10	08X25Z16	21'-1 1/2"
1	G-11	08X25Z14	21'-1 1/2"
2	CB-5	CABLE500	24'-5 7/8"

CONNECTION PLATES FRAME LINE A		
QTY	MARK	PART
2	4	C2



SIDEWALL FRAMING: FRAME LINE A



SIDEWALL SHEETING & TRIM: FRAME LINE A
PANELS: 26 Ga. R - Charcoal Grey

GENERAL NOTES:

1. Use TEK5WW screws in place of SD150 panel screws at all 10 gage members.
2. All connections to door or window jambs where the clip is not designated in the clip table / drawing are made with JC# clips (#= Girt Depth).

DRAWING IS NOT TO SCALE

TRIM COLORS	
EAVE TRIM = SP Black	CORNER TRIM = SP Black
BASE TRIM = Charcoal Grey	GUTTER = SP Black
DOOR TRIM = SP Black	DOWNSPOUTS = SP Black
RAKE TRIM = SP Black	
* LINER TRIM = Liner panel color	
* SOFFIT TRIM = Soffit panel color	

* ONLY APPLICABLE IF LINER TRIM OR SOFFIT PANEL IS INDICATED ON BUILDING ORDER.



JAMES ROBERT & JOSE ROBERT
Olympic Steel Buildings
888-449-7756
JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
DATE: 6/19/25 REVISION: 0
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JAMES ROBERT & JOSE ROBERT
CITY: CHELMSFORD ST./P.V. ON

REV	DESCRIPTION	DATE

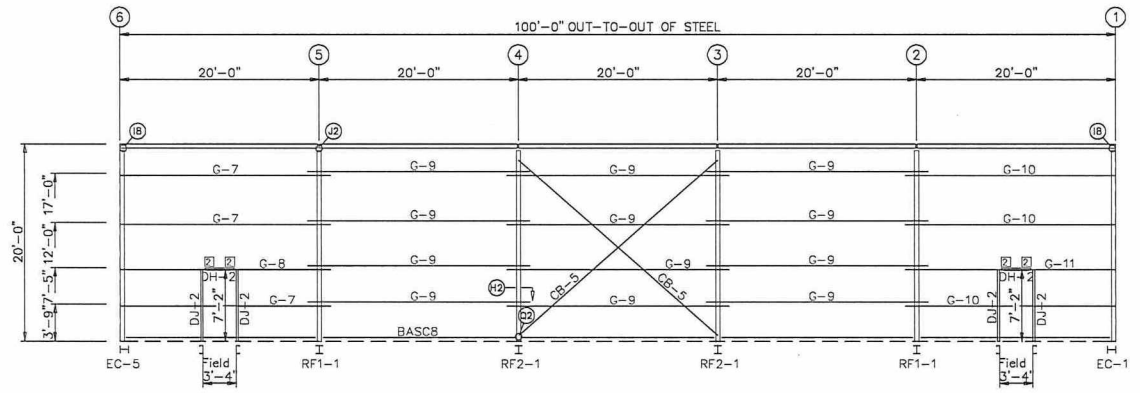
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SKETCH 14

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REGISTERED PROFESSIONAL ENGINEER
A. IBRAHIM
90463316
07/15/25
PROVINCE OF ONTARIO

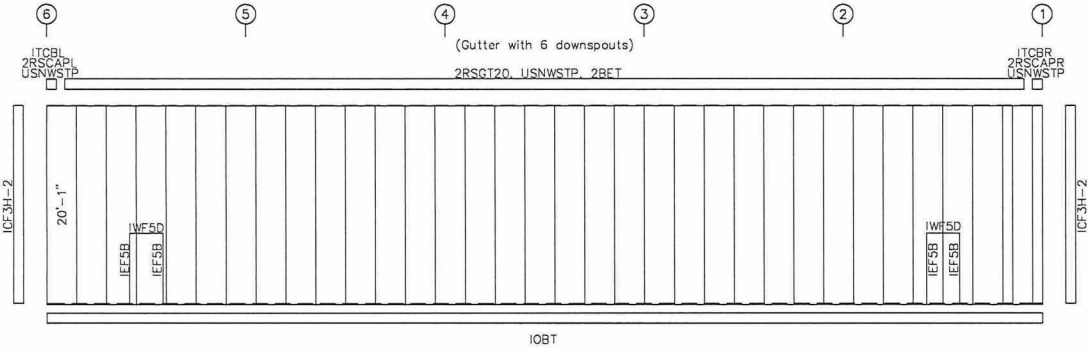
MEMBER TABLE			
FRAME LINE E			
QUAN	MARK	PART	LENGTH
4	DJ-2	08X35C16	7'-4 3/4"
2	DH-2	08X30C16	3'-4"
3	G-7	08X25Z16	21'-1 1/2"
1	G-8	08X25Z14	21'-1 1/2"
12	G-9	08X25Z16	22'-3 1/2"
3	G-10	08X25Z16	21'-1 1/2"
1	G-11	08X25Z14	21'-1 1/2"
2	GB-5	CABLE500	24'-5 7/8"

CONNECTION PLATES		
FRAME LINE E		
ID	QUAN	MARK/PART
2	4	c2



GIRT LAPS 1'-1 3/4" 1'-1 3/4" 1'-1 3/4" 1'-1 3/4" 1'-1 3/4" 1'-1 3/4"

SIDEWALL FRAMING: FRAME LINE E



SIDEWALL SHEETING & TRIM: FRAME LINE E
PANELS: 26 Ga. R - Charcoal Grey

GENERAL NOTES:
 1. Use TEK5WW screws in place of SD150 panel screws at all 10 gage members.
 2. All connections to door or window jambs where the clip is not designated in the clip table / drawing are made with JC# clips (#= Girt Depth).

DRAWING IS NOT TO SCALE

TRIM COLORS			
EAVE TRIM = SP Black	CORNER TRIM = SP Black	BASE TRIM = Charcoal Grey	GUTTER = SP Black
DOOR TRIM = SP Black	DOWNSPOUTS = SP Black	RAKE TRIM = SP Black	
LINER TRIM = Liner panel color		SOFFIT TRIM = Soffit panel color	
* ONLY APPLICABLE IF LINER TRIM OR SOFFIT PANEL IS INDICATED ON BUILDING ORDER.			



Olympia Steel Buildings
 888-449-7756

JAMES ROBERT & JOSE ROBERT
 60'-0" x 100'-0" x 20'-0"
 DATE: 6/19/25 REVISION: 0
 ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

JAMES ROBERT & JOSE ROBERT
 CITY: CHELMSFORD ST/PR: ON

REV	DESCRIPTION	DATE

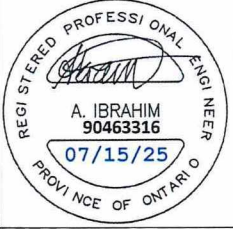
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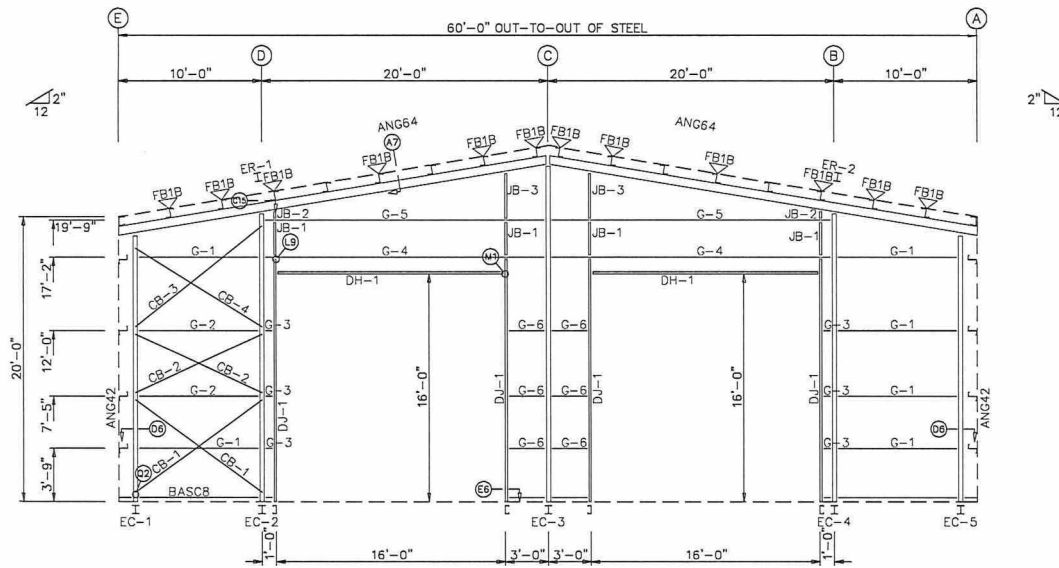
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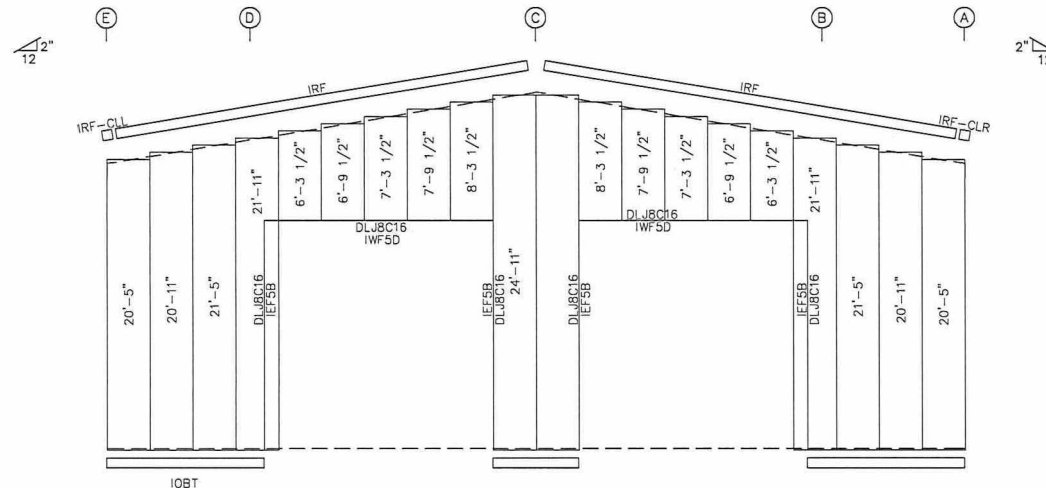
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PL-MV-2025-00118
SKETCH 15





ENDWALL FRAMING: FRAME LINE 1



ENDWALL SHEETING & TRIM: FRAME LINE 1
PANELS: 26 Ga. R - Charcoal Grey

GENERAL NOTES:

1. Use TEK5WW screws in place of SD150 panel screws at all 10 gage members.
2. See detail C7A for field coping of coldform endwall column flange braces.
3. All connections to door or window jambs where the clip is not designated in the clip table / drawing are made with JC# clips (# = Girt Depth).

BOLT TABLE FRAME LINE 1				
LOCATION	QUAN	TYPE	DIA	LENGTH
ER-1/ER-2	8	A325	1/2"	1 1/2"
Cor_Column/Raf	2	A325	3/4"	1 3/4"
EC-2/ER-1	2	A325	3/4"	1 3/4"
EC-3/ER-2	2	A325	1/2"	1 3/4"
EC-4/ER-2	2	A325	3/4"	1 3/4"

MEMBER TABLE FRAME LINE 1			
QUAN	MARK	PART	LENGTH
1	EC-1	W8X10	18'-5 5/16"
1	EC-2	W8X10	19'-11"
1	EC-3	W8X24	23'-0 5/8"
1	EC-4	W8X10	19'-11"
1	EC-5	W8X10	18'-5 5/16"
1	ER-1	W12X16	30'-4 15/16"
1	ER-2	W12X16	30'-4 15/16"
1	DJ-1	08X35C13	17'-1 3/4"
2	DJ-1	08X35C16	16'-0"
2	G-1	08X25Z16	1'-5 1/2"
2	G-2	08X25Z13	1'-5 1/2"
2	G-3	08X25Z16	1'-5 1/2"
2	G-4	08X25Z13	1'-5 1/2"
2	G-5	08X25Z16	1'-5 1/2"
2	G-6	08X25Z16	1'-5 1/2"
2	CB-1	CABLE250	10'-11 5/8"
2	CB-2	CABLE250	7'-8 1/16"
2	CB-3	CABLE250	10'-2 11/16"
2	CB-4	CABLE250	10'-5 11/16"
1	JB-1	08X35C16	2'-6 1/2"
4	JB-2	08X35C16	2'-6 1/2"
2	JB-3	08X35C16	2'-8 1/2"

FLANGE BRACE TABLE FRAME LINE 1		
VID	MARK	LENGTH
1	FB1B	1'-5 1/8"



JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
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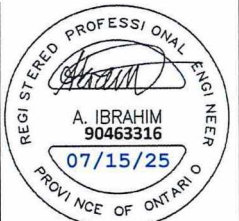
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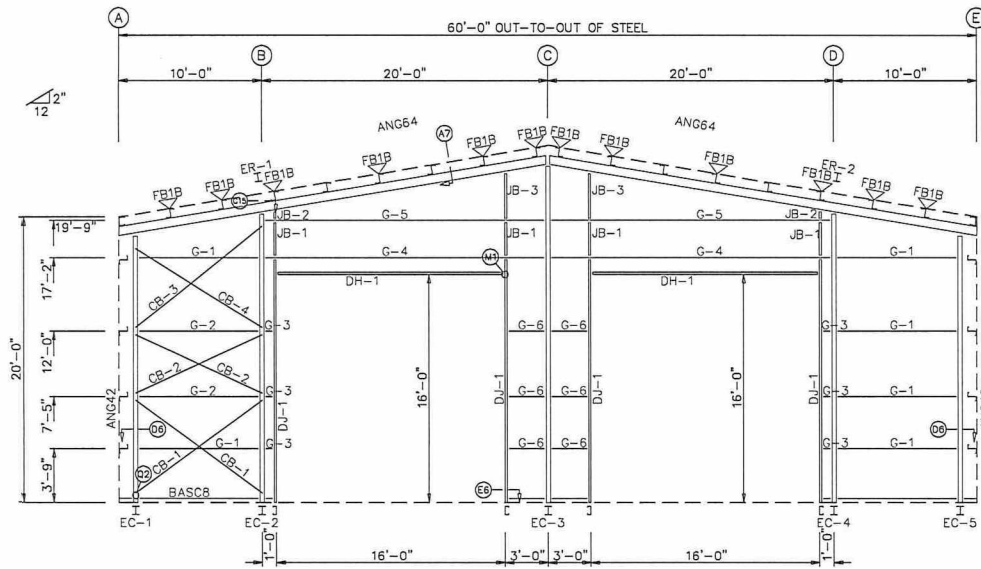
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FINAL DRAWINGS.

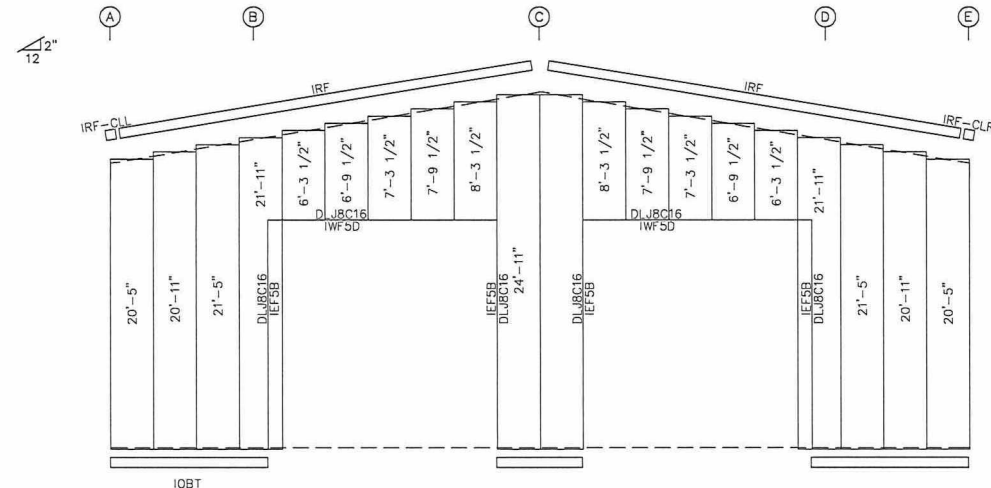
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TRIM COLORS			
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DOOR TRIM = SP Black	DOWNSPOUTS = SP Black	RAKE TRIM = SP Black	
* LINER TRIM = Liner panel color		* SOFFIT TRIM = Soffit panel color	
* ONLY APPLICABLE IF LINER TRIM OR SOFFIT PANEL IS INDICATED ON BUILDING ORDER.			





ENDWALL FRAMING: FRAME LINE 6



ENDWALL SHEETING & TRIM: FRAME LINE 6
PANELS: 26 Ga. R - Charcoal Grey

BOLT TABLE FRAME LINE 6				
LOCATION	QUAN	TYPE	DIA	LENGTH
ER-1/ER-2	8	A325	1/2"	1 1/2"
Cor_Column/Raf	2	A325	3/4"	1 3/4"
EC-2/ER-1	2	A325	3/4"	1 3/4"
EC-3/ER-2	2	A325	1/2"	1 3/4"
EC-4/ER-2	2	A325	3/4"	1 3/4"

MEMBER TABLE FRAME LINE 6			
QUAN	MARK	PART	LENGTH
1	EC-1	W8X10	18'-5 5/16"
1	EC-2	W8X10	19'-11"
1	EC-3	W8X24	23'-0 5/8"
1	EC-4	W8X10	19'-11"
1	EC-5	W8X10	18'-5 5/16"
1	ER-1	W12X16	30'-4 15/16"
1	ER-2	W12X16	30'-4 15/16"
4	DJ-1	08X35C13	17'-1 3/4"
2	DH-1	08X35C16	16'-0"
6	G-1	08X25Z16	16'-5 1/2"
6	G-2	08X25Z13	15'-1 1/2"
6	G-3	08X25Z16	16'-0"
6	G-4	08X25Z13	16'-6 1/4"
6	G-5	08X25Z16	19'-6 1/4"
6	G-6	08X25Z16	2'-4 3/4"
2	CB-1	CABLE250	11'-5 8/8"
2	CB-2	CABLE250	7'-8 1/16"
1	CB-3	CABLE250	11'-5 8/8"
1	CB-4	CABLE250	11'-5 8/8"
4	JB-1	08X35C16	2'-6 1/2"
2	JB-2	08X35C16	1'-5 1/8"
2	JB-3	08X35C16	2'-6 1/2"

FLANGE BRACE TABLE FRAME LINE 6		
VIDI MARK	LENGTH	
1 FB1B	1'-5 1/8"	



JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
DATE: 6/19/25 REVISION: 0
ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

JAMES ROBERT & JOSE ROBERT
CITY: CHELMSFORD ST/PV: ON

REV	DESCRIPTION	DATE

PL-MV-2025-00118
SKETCH 17

DRAWING STATUS

FOR APPROVAL: THESE DRAWINGS, BEING FOR APPROVAL, ARE BY DEFINITION NOT FINAL, AND ARE FOR CONCEPTUAL REPRESENTATION ONLY. THE PROJECT DOCUMENTS, ONLY DRAWINGS ISSUED FOR CONSTRUCTION, CAN BE CONSIDERED AS COMPLETE.

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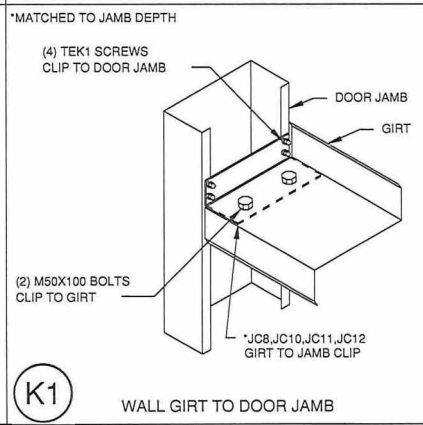
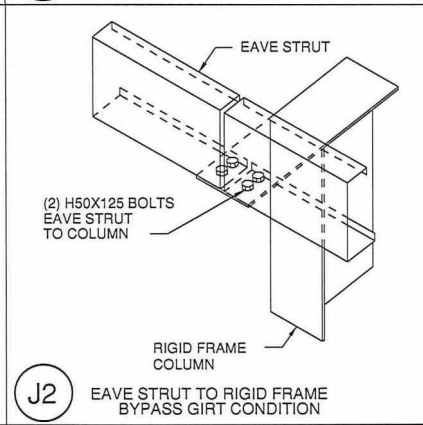
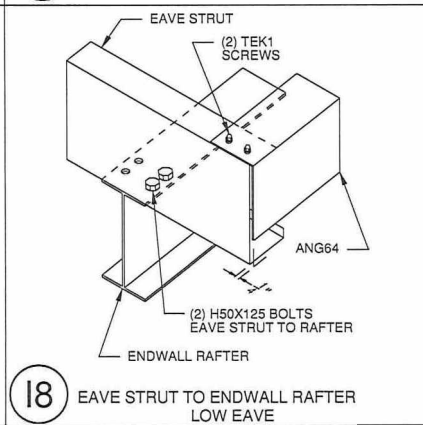
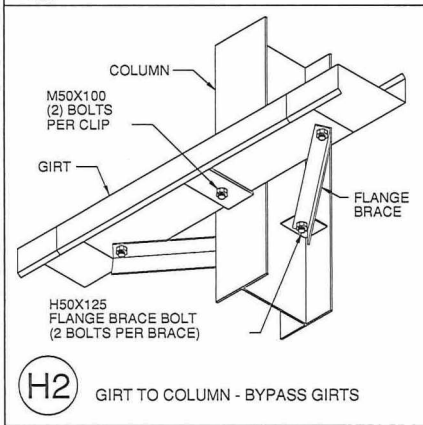
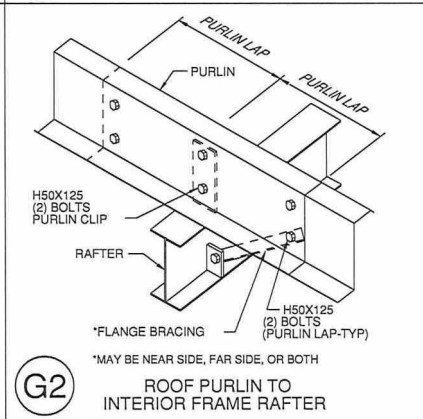
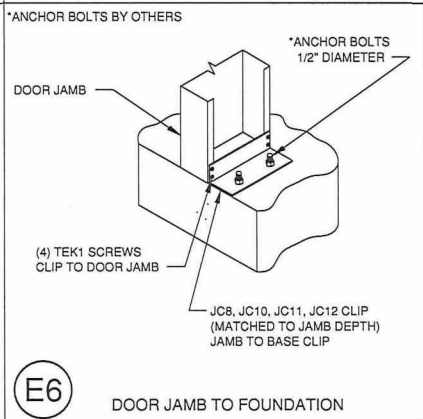
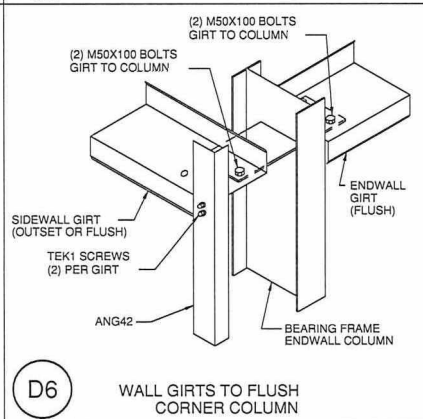
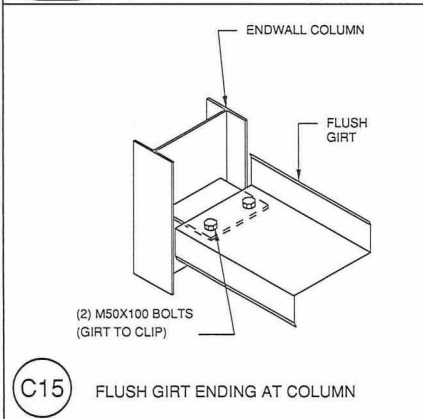
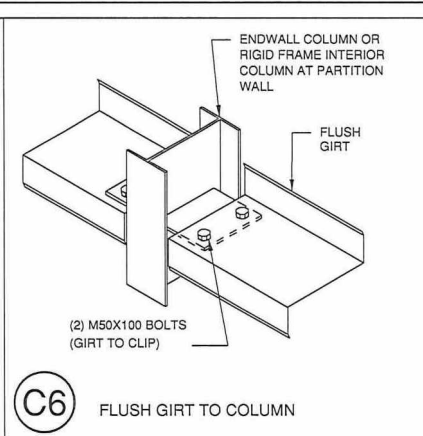
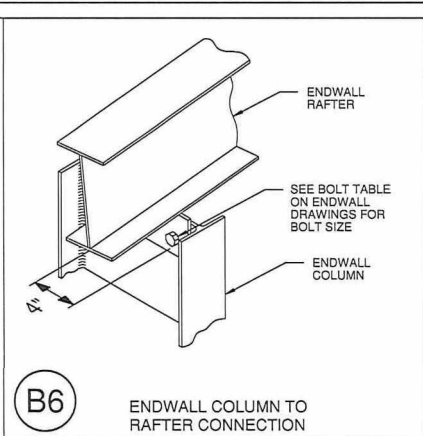
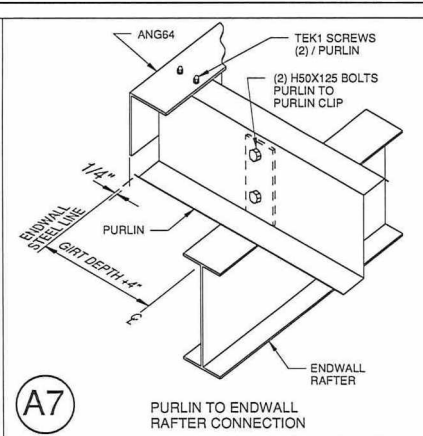
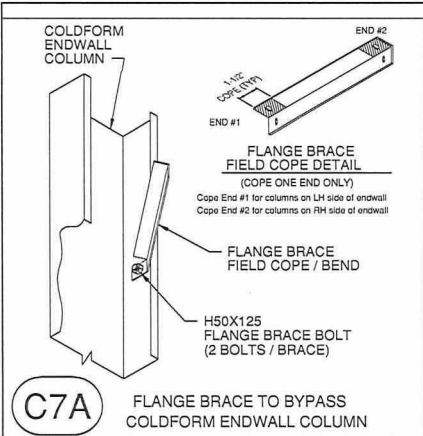
REGISTERED PROFESSIONAL ENGINEER
A. IBRAHIM
90463316
07/15/25
PROVINCE OF ONTARIO

DRAWING IS NOT TO SCALE

TRIM COLORS			
EAVE TRIM = SP Black	CORNER TRIM = SP Black	BASE TRIM = Charcoal Grey	GUTTER = SP Black
DOOR TRIM = SP Black	DOWNSPOUTS = SP Black	RAKE TRIM = SP Black	
LINER TRIM = Liner panel color		SOFFIT TRIM = Soffit panel color	
* ONLY APPLICABLE IF LINER TRIM OR SOFFIT PANEL IS INDICATED ON BUILDING ORDER.			

GENERAL NOTES:

1. Use TEK5WW screws in place of SD150 panel screws at all 10 gage members.
2. See detail C7A for field coping of coldform endwall column flange braces.
3. All connections to door or window jambs where the clip is not designated in the clip table / drawing are made with JC# clips (#= Girt Depth).

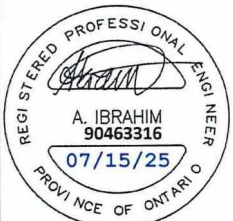


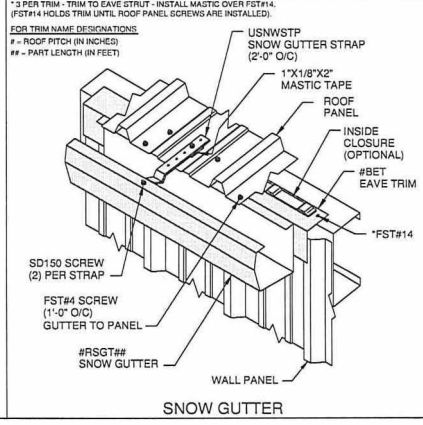
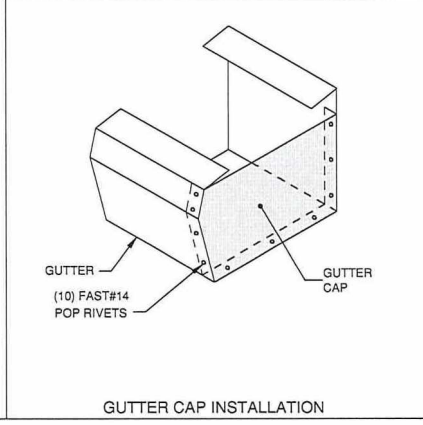
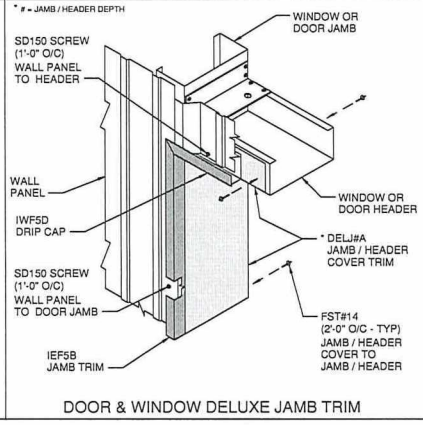
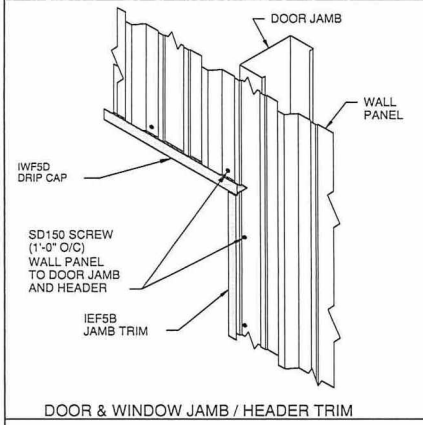
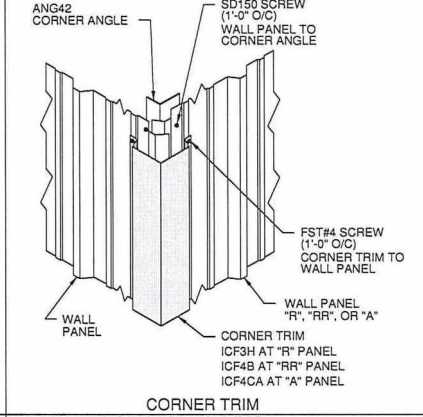
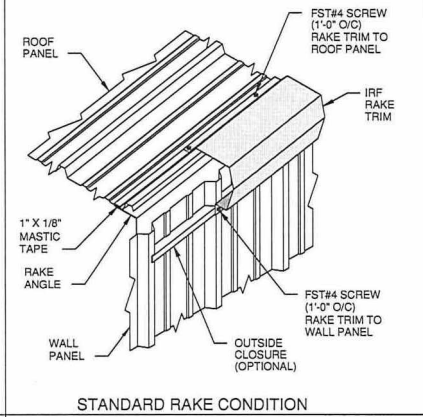
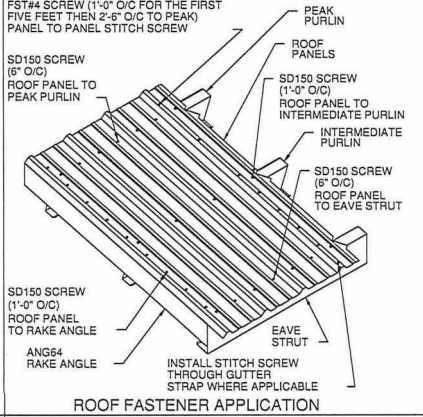
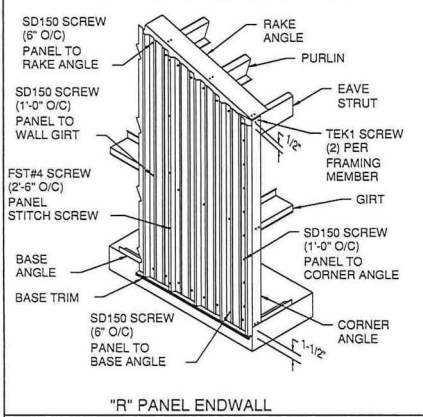
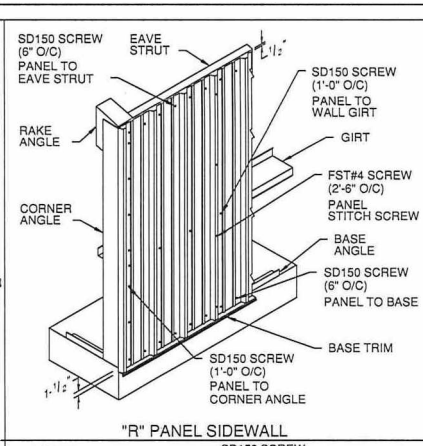
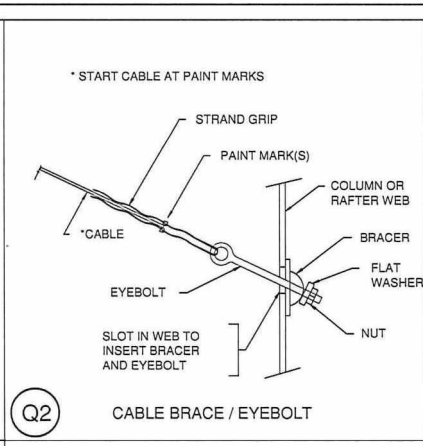
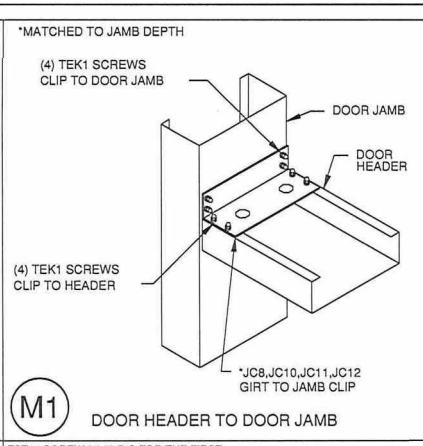
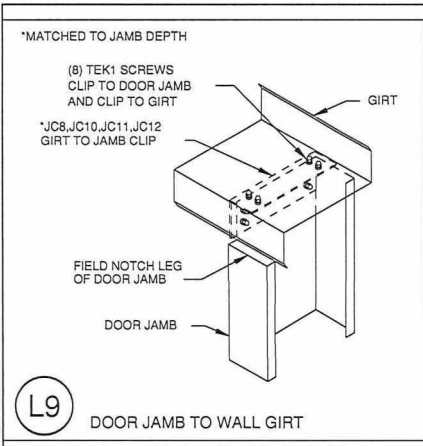
Olympia Steel Buildings
888-449-7756

JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
DATE: 6/19/25 REVISION: 0
ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

JAMES ROBERT & JOSE ROBERT CITY: CHELMSFORD ST/PV: ON		REVISION HISTORY	DATE
		DESCRIPTION	
DRAWING STATUS		PL-MV-2025-00118 SKETCH 18	
FOR APPROVAL: THESE DRAWINGS, BEING FOR APPROVAL, ARE BY DEFINITION NOT FINAL, AND ARE FOR CONCEPTUAL REPRESENTATION ONLY. THE PROJECT DOCUMENTS, ONLY DRAWINGS ISSUED "FOR CONSTRUCTION" CAN BE CONSIDERED AS COMPLETE.		FOR PERMIT: THESE DRAWINGS, BEING FOR PERMIT, ARE BY DEFINITION NOT FINAL, AND ARE FOR CONSTRUCTION ONLY. FOR CONSTRUCTION: FINAL DRAWINGS.	





OLYMPIA STEEL BUILDINGS
 Olympia Steel Buildings
 888-449-7756

JAMES ROBERT & JOSE ROBERT
 60'-0" x 100'-0" x 20'-0"

DATE: 6/19/25 | REVISION: 0
 ENG: CJA | DWN: BJC | APPD: CJA

F.O. 30302

REV#	DESCRIPTION	DATE

JAMES ROBERT & JOSE ROBERT
 CITY: CHELMSFORD ST/PR: ON

DRAWING STATUS

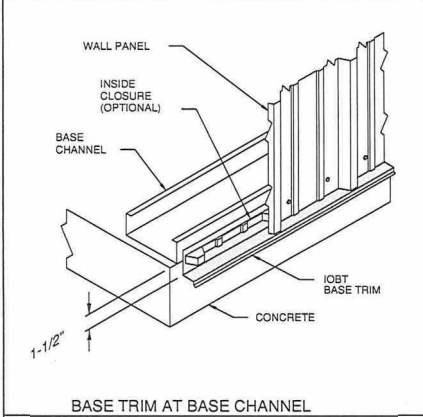
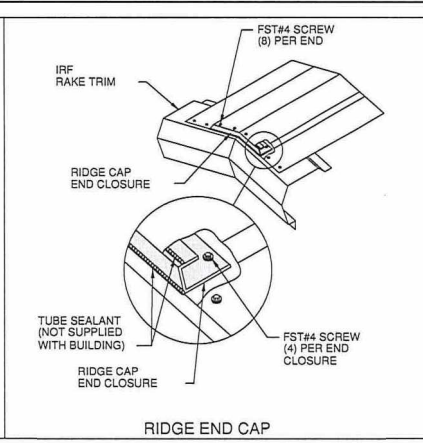
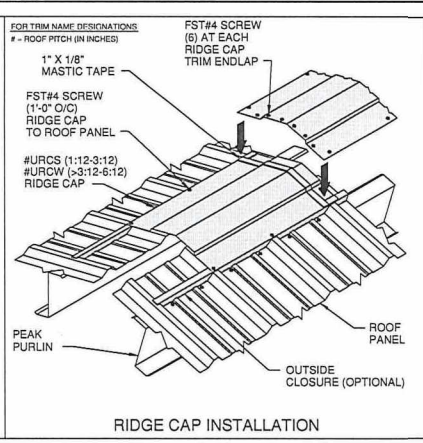
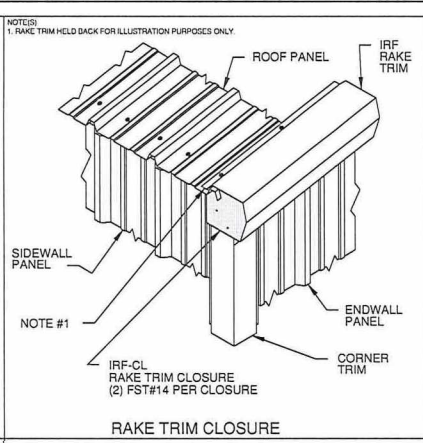
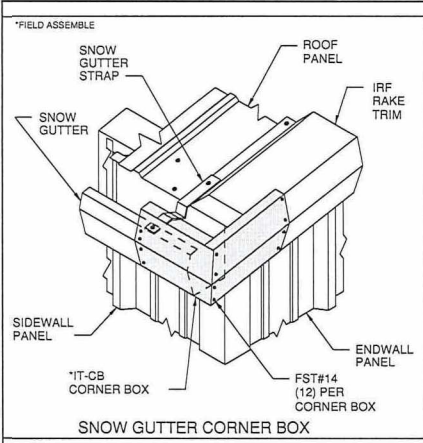
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FOR CONSTRUCTION: FINAL DRAWINGS.

PL-MV-2025-00118
 SKETCH 19

REGISTERED PROFESSIONAL ENGINEER
 A. IBRAHIM
 90463316
 07/15/25
 PROVINCE OF ONTARIO



Olympia Steel Buildings
888-449-7756

JAMES ROBERT & JOSE ROBERT
60'-0" x 100'-0" x 20'-0"
DATE: 6/19/25 REVISION: 0
ENG: CJA DWN: BJC APPD: CJA

F.O. 30302

REV	DESCRIPTION	DATE

JAMES ROBERT & JOSE ROBERT
CITY: CHELMSFORD ST.PVY: ON

DRAWING STATUS

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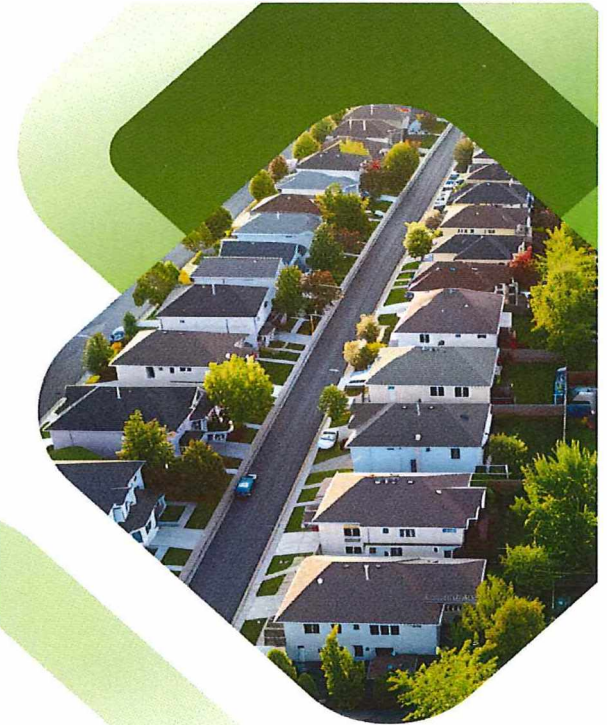
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PL-MV-2025-00118
SKETCH 20

4176 RR 35
CHELMSFORD

PIN 733471438

GeoWarehouse Property Report



Property Details

GeoWarehouse Address:

4176 RR 35

CHELMSFORD

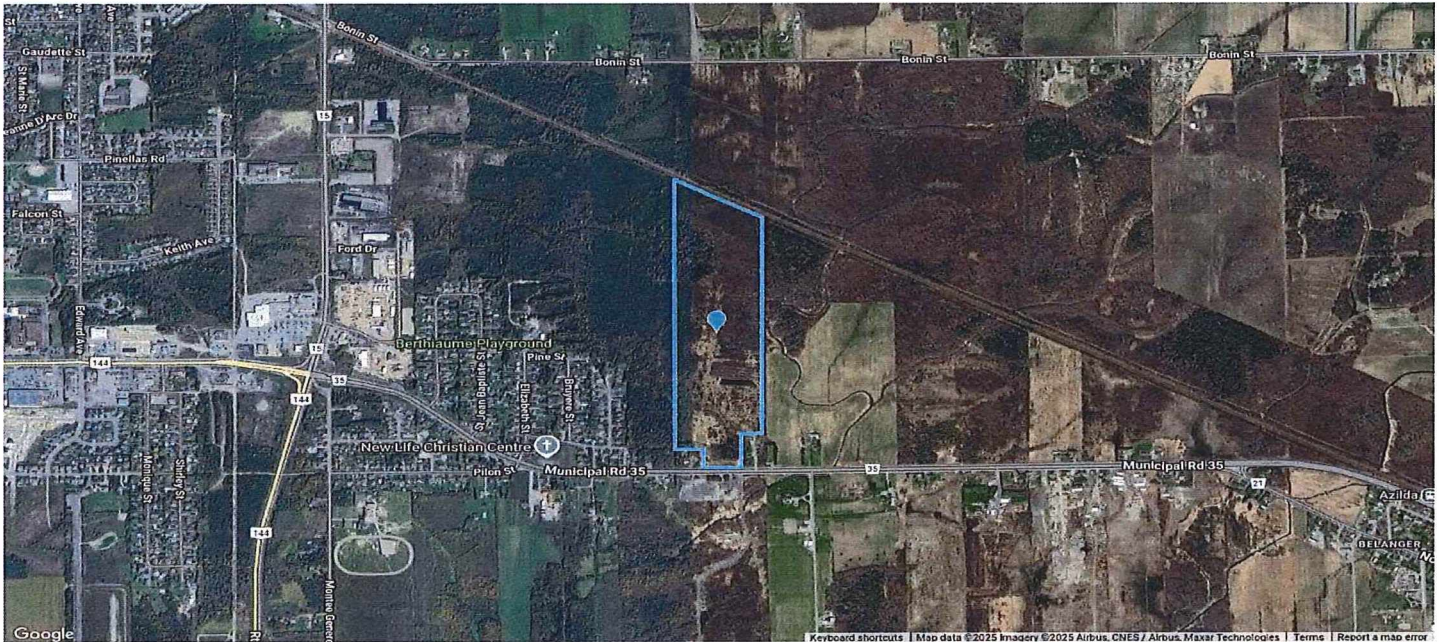
PIN: 733471438

Land Registry Office: SUDBURY (53)

Land Registry Status: Active

Registration Type: Certified (Land Titles)

Ownership Type: Freehold



Ownership

Owner Name:

ROBERT, JOSEE; ROBERT, JAMES

Legal Description

SURFACE RIGHTS ONLY: PT LT 10 CON 3 RAYSIDE AS IN LT19549 EXCEPT LT60738, LT134494, PTS 4, 5, 6 & 7 53R18049; T/W EASEMENT OVER PT LT 10 CON 3, PT 5 53R18049 AS IN SD111689; S/T LT124264 & LT507116; GREATER SUDBURY

Lot Size

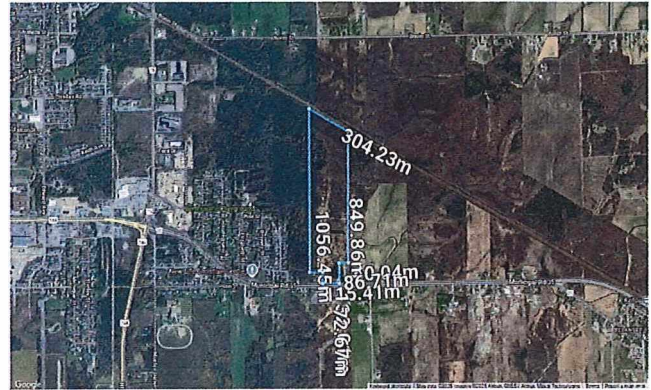
Area: 265458.0 sq.m

Perimeter: 2677.0 m

Measurements: 115.41m x 132.01m x 60.04m x 849.86m x
304.23m x 1056.45m x 86.71m x 72.67m

Lot Measurement Accuracy : LOW

These lot boundaries may have been adjusted to fit within the overall parcel fabric and should only be considered to be estimates.



Sales History

Sale Date	Type	Party To	Notes
Sep 21, 2023	Transfer by Personal Representative	ROBERT, JOSEE; ROBERT, JAMES;	See Notes 1
Jun 01, 2017	Transmission by Personal Representative (Land)	LAAKSOHARJU, PENTTI - ESTATE; SAARI, KLAUS HENRIK; LAAKSO, PIRLLO KAARINA;	
Aug 20, 1952	Transfer	LAAKSOHARJU, PENTTI; LAAKSOHARJU, VIENO ELISABETH;	

Notes :

1. The following Pins were transferred together with the subject Property

733471440

Terms and Conditions

Reports Not the Official Record. Reports, other than the Parcel Register, obtained through Geowarehouse are not the official government record and will not necessarily reflect the current status of interests in land.

Currency of Information. Data contained in the Geowarehouse reports are not maintained real-time. Data contained in reports, other than the Parcel Register, may be out of date ten business days or more from data contained in POLARIS.

Coverage. Data, information and other products and services accessed through the Land Registry Information Services are limited to land registry offices in the areas identified on the coverage map.

Completeness of the Sales History Report. Some Sales History Reports may be incomplete due to the amount of data collected during POLARIS title automation. Subject properties may also show nominal consideration or sales price (e.g. \$2) in cases such as transfers between spouses or in tax exempt transfers.

Demographic Information. Demographic Information is obtained from Environics Analytics. Environics Analytics acquires and distributes Statistics Canada files in accordance with the Government of Canada's Open Data Policy. No information on any individual or household was made available to Environics Analytics by Statistics Canada. PRIZM and selected PRIZMC2 nicknames are registered trademarks of The Nielsen Company (U.S.) and are used with permission.

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Parcel Mapping shown on the site was compiled using plans and documents recorded in the Land Registry System and has been prepared for property indexing purposes only. It is not a Plan of Survey. For actual dimensions of property boundaries, see recorded plans and documents.



Box 5000, Station A
200 Brady Street
Sudbury, Ontario P3A 5P3
(705) 671-2489 ext 4376 or 4346
(705) 673-2200 FAX

REVISED

Record #: PL-MV-2025-00072

APPLICATION SUMMARY

File Date: 05/27/2025

Application Type: Minor Variance

Address(es): 1827 Torbay Road, Sudbury P3B 1A3

Applicant(s): PROSPEC ENGINEERING AND ARCHITECTURE

Owner(s): SAHIL BHARDWAJ AND NEELIMA BHARDWAJ

PLANNING APPLICATION

Are there multiple properties associated with the application?

No

Please describe the additional properties associated with this application

What is the date the current Owner(s) acquired the property?

2021/03/21

Are you the registered owner or an authorized agent?

Registered Owner

What is the number of dwelling units on the property?

1

What is the number of proposed new dwelling units on the property?

0

What is the number of proposed new buildings/structures on the property?

0

What is the number of existing buildings/structures on the property?

3

If this application is approved, would any existing dwelling units be legalized?

No

How many dwelling units will be legalized?

Is this property located within an area subject to the Greater Sudbury Source Protection Plan?

Yes

Provide details on how the property is designated in the Source Protection Plan

Source Protection Area: Nickel District

Current Official Plan designation

Living Area I

Current Official Plan designation (additional)

Current Zoning By-law designation

R1-5

Provide a detailed description of what is being proposed

Sunroom Addition

Provide a detailed reason why the proposal cannot comply with the Zoning By-law

Adding a sunroom won't effect the zoning, as its not a additional residential or dwelling being added, it is an additional structure on side of the house as additional house amenity.

As rationale for the requested relief from the high water mark setback for the addition; The addition has been proposed on the South-Western corner of the existing detached dwelling such that it makes use of, and does not conflict with, existing openings in the structure from the living room. To the north, two sliding glass doors exist, one from the upstairs bedroom and the other from the ground floor kitchen that are not oportune locations for this space. If the addition were to be located on the South-East facade that would conflict with the existing chimney, a bedroom, or the side-yard setback. In place of another gazebo structure that is independent from the dwelling, the homeowner preferred an attached space that could be accessed from their living room. As a result, the elevated design affords a low-impact construction on helical piles that only has 3 additional points of contact with the ground, allowing for naturalized plantings below.

As rationale for the requested relief from the high water mark setback for Garden Shed A; This structure - and Garden Shed B - was built without the benefit of building permit prior to the current owner's acquisition of this property. At the committee's discretion we are hoping that the documentation of the existing structure, removal of Garden Shed B and re-naturalization of the surrounding hardscape in that vicinity will be sufficient for the relief of this essential structure that services the rear yard of the property.

Is there an eave encroachment?

No

Size of eaves

Lot Frontage of the property

19.0

Lot Depth of the property

79.78

Lot Area of the property

1497.2

Total width of the public road giving access to the property

13.5

List all buildings and structures on the property and their respective date of construction

Dwelling

Garden Shed A

Garden Shed B

Existing use(s) of the subject property and length of time it/they have continued, ex. Residential, Commercial, Institutional, Park, etc.

Residential

Is the use remaining the same? If no, please provide the proposed new use

SAME

Existing uses of neighbouring properties

Residential

Has the property ever been subject of a previous application for minor variance/permission?

No

Do you require zoning relief from Section 4.41 (Waterbodies – Water Frontage, Setbacks and Buffers)?

Yes

Have you consulted with the Strategic and Environmental Planning department regarding this relief?

No

Have you consulted with Conservation Sudbury regarding this relief?

No

WATER SUPPLY AND SEWAGE DISPOSAL

- Municipally owned and operated piped water system
- Municipally owned and operated sanitary sewage system
- Lake
- Pit Privy
- Individual Well
- Communal Well
- Individual Septic System
- Communal Septic System
- Other
- Explain Other

PROPERTY ACCESS

- Provincial highway
- Right-of-way
- Municipal road that is maintained seasonally
- Municipal road that is maintained year-round
- Water

Indicate the parking and docking facilities to be used if via water

Estimate the distance of these facilities from the retained land and nearest public road by water

CONCURRENT APPLICATIONS

Is the property the subject of a current application for Consent?
No

Indicate the application number(s) and status of the application(s)

Is the property the subject of a current application for a Plan of Subdivision?
No

Indicate application number(s) and application status

PROPOSED BUILDING/STRUCTURE

Building Description	Same As Existing	Proposed Ground Floor Area (m2)	Proposed Gross Floor Area (m2)	Proposed Number of Storeys	Proposed Width (m)	Proposed Length (m)	Proposed Height (m)	Proposed Front Yard Setback (m)	Proposed Rear Yard Setback (m)	Proposed Side Yard Setback (m)	Proposed Side Yard Setback Other (m)
Addition of Sunroom on rear left side of the house as per the attached drawing	No	14.64	14.64	1	2.4	6.1	4.75	44.2	27.2	14.0	3.3

EXISTING BUILDING/STRUCTURE

Building Description	To Be Demolished	Existing Ground Floor Area (m2)	Existing Gross Floor Area (m2)	Existing Number of Storeys	Existing Width (m)	Existing Length (m)	Existing Height (m)	Existing Front Yard Setback (m)	Existing Rear Yard Setback (m)	Existing Side Yard Setback (m)	Existing Side Yard Setback Other (m)
Residential lake side single family house	No	242.34	242.34	1	15.3	21.6	4.75	28.4	26.4	1.5	2.2
Garden Shed A	No	18.09	18.09	1	4.9	3.7	3.96	55.6	21.1	1.2	15.5
Garden Shed B	Yes	10.43	10.43	1	2.9	3.7	3.96	61.6	15.9	1.6	17.7

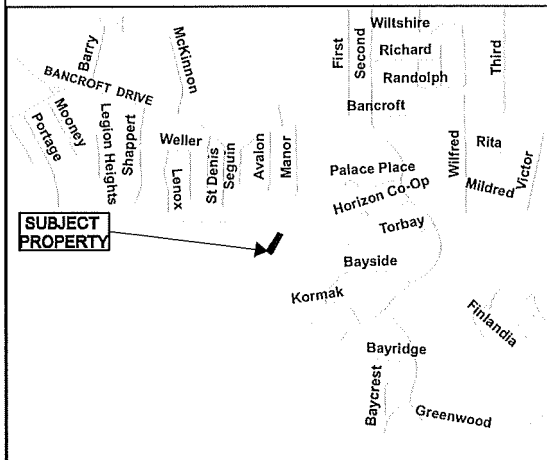
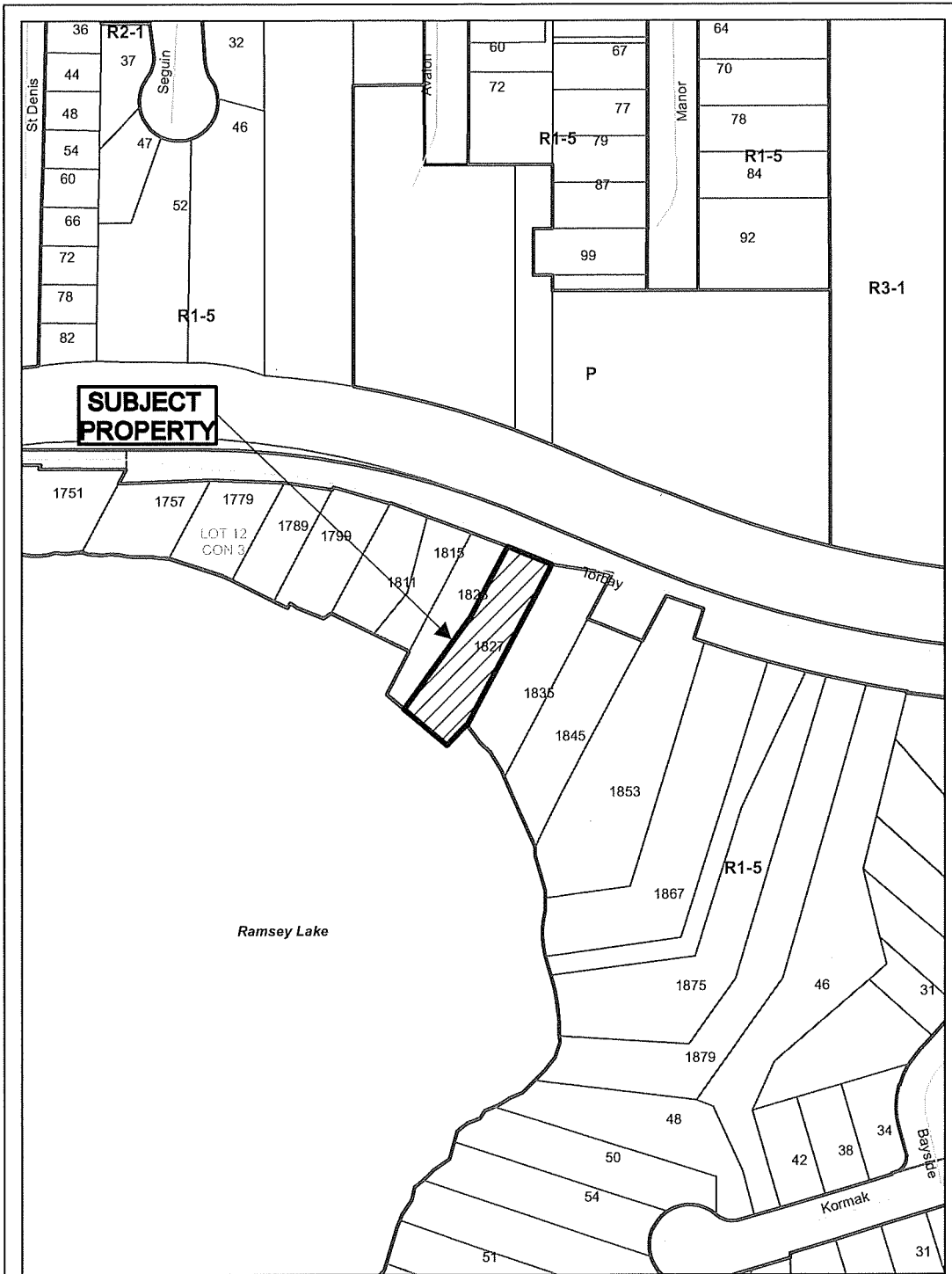
ZONING BY-LAW RELIEF

Variance To	By-law Requirement	Proposed (m)	Difference (m)
Addition of Sunroom - Subsection 4.41.2	30.00	17.9	12.1
Addition of Sunroom - Subsection 4.41.4	20.0	17.9	2.1
Garden Shed A - Subsection 4.41.2	30.0	13.7	6.3
Garden Shed A - Subsection 4.41.4	20.0	13.7	6.3

PL-MV-2025-00072 – 1827 Torbay Road, Sudbury

In response to the comments received and subject to the committee's decision:

- Upon further site verification, the dimensions and location(s) of each Garden shed have been updated to reflect the existing structures. The application can be revised to match that of the new plot plan.
 - Garden Shed A is still larger than 15 m² (18.09 m²) and the owner of the property will submit a building permit for this structure.
 - Garden Shed B is only 10.49 m² and the structure's eaves extend past the side yard lot line, but the building face does not.
- To satisfy the shoreline buffer area requirements, we intend to removing Garden Shed B and the surrounding hardscaping to restore the natural vegetated state to 75% within the buffer area.
- Permit/approval will be sought from Conservation Sudbury at the time of the permit application.
- As rationale for the requested relief from the high water mark setback for the addition; The addition has been proposed on the South-Western corner of the existing detached dwelling such that it makes use of, and does not conflict with, existing openings in the structure from the living room. To the north, two sliding glass doors exist, one from the upstairs bedroom and the other from the ground floor kitchen that are not opportune locations for this space. If the addition were to be located on the South-East facade that would conflict with the existing chimney, a bedroom, or the side-yard setback. In place of another gazebo structure that is independent from the dwelling, the homeowner preferred an attached space that could be accessed from their living room. As a result, the elevated design affords a low-impact construction on helical piles that only has 3 additional points of contact with the ground, allowing for naturalized plantings below.
- As rationale for the requested relief from the high water mark setback for Garden Shed A; This structure - and Garden Shed B - was built without the benefit of building permit prior to the current owner's acquisition of this property. At the committee's discretion we are hoping that the documentation of the existing structure, removal of Garden Shed B and re-naturalization of the surrounding hardscape in that vicinity will be sufficient for the relief of this essential structure that services the rear yard of the property.



Application for Minor Variance or Permission



Subject Property being PIN 73578-0119,
 Parcel 5878 SEC SES,
 Part Lot 12, Concession 3,
 Parts 11-13, Plan 53R-13566,
 together with easement over Parts 2, 3, 5, 6, 8 & 10,
 Plan 53R-13566, Township of Neelon,
 1827 Torbay Road, Sudbury,
 City of Greater Sudbury

Sketch 1, NTS
 NDCA

PL-MV-2025-00072
 Date: 2025 06 04

SAVY BHARDWAJ

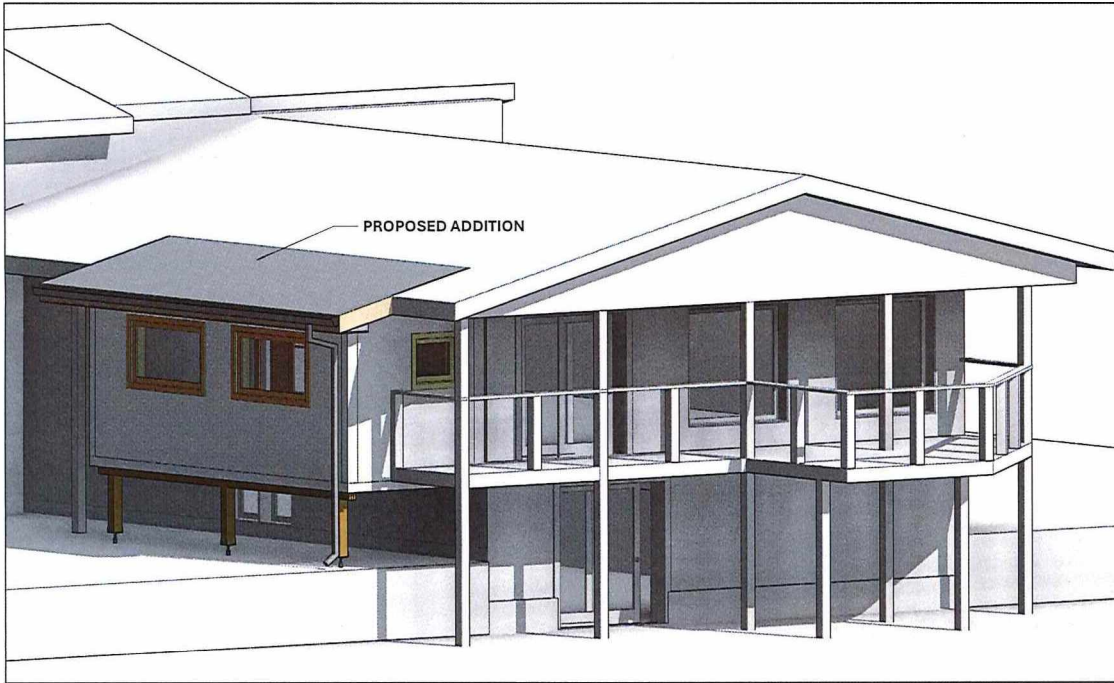
1827 TORBAY ROAD,
SUDBURY, ON
SUNROOM ADDITION
PRO-25-084
ISSUED FOR MINOR VARIANCE
2025-08-15



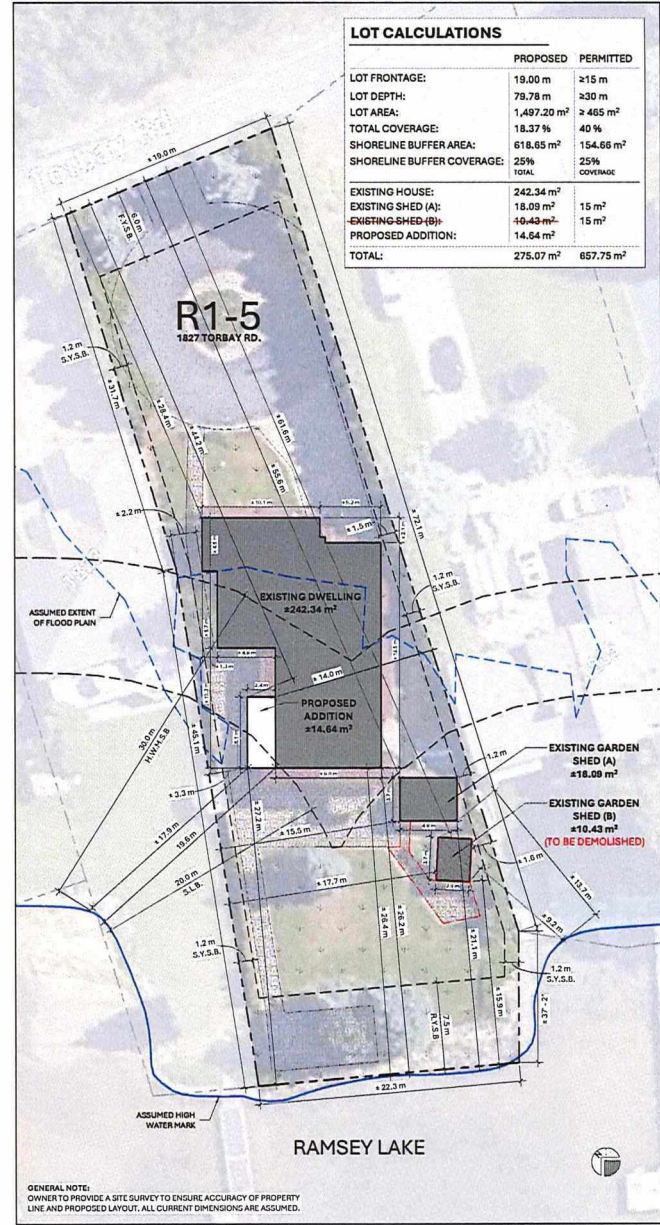
SITEPLAN LEGEND	
GRAPHIC	DESCRIPTION
	PROPERTY LINE
	ADJACENT PROPERTY LINE
	SETBACK LINE
	HIGH WATER MARK
	FLOOD PLAIN
	DENOTES NEW BUILDING
	DENOTES NATURALIZED AREA
	DENOTES EXISTING ASPHALT
	DENOTES EXISTING PATIO
	DENOTES EXISTING TO BE RE-NATURALIZED

ABBREVIATIONS	
ABBREVIATION	DESCRIPTION
S.Y.S.B	SIDE YARD SETBACK
F.Y.S.B	FRONT YARD SETBACK
R.Y.S.B	REAR YARD SETBACK
H.W.M.S.B	HIGH-WATER MARK SETBACK
H.W.M	HIGH-WATER MARK
S.L.B	SHORELINE BUFFER

1 SITE PLAN LEGEND
1/16" = 1'-0"



2 PROJECT PERSPECTIVE



1 PLOT PLAN
1/16" = 1'-0"

SIGN & SEAL

PROSPEC
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PL-MV-2025-00072 SKETCH 2

REVISIONS

#	DESCRIPTION	DATE
0	ISSUED FOR MINOR VARIANCE	2025-08-15

PRO-25-084
SUNROOM ADDITION
1827 TORBAY ROAD, SUDBURY, ON
CLIENT: SAVY BHARDWAJ

PLOT PLAN

DRAWN BY: SB
CHECKED BY: BH
APPROVED BY: EL
SCALE: 1/16" = 1'-0"
SHEET
A001
REVISION 0